



D E S I G N
R E V I E W
C O M M I T T E E
S T A F F R E P O R T

For Advisory Comments
FEBRUARY 17, 2022

747 (787) Bancroft Way PREVIEW

Design Review #DRCP2021-0012 to demolish six existing buildings and construct a 162,375 square-foot, three-story building containing 133,028 square feet of research and development space and 29,347 square feet of light manufacturing space, and a surface parking lot containing 75 off-street parking spaces and five loading spaces.

I. Introduction

This project is located on the Northwest corner of Bancroft and Fifth in the MU-LI district of West Berkeley. The project site would merge four parcels for the construction of a three-story research and development building.

The demolition of six vacant one-story non-residential structures is being scheduled on an upcoming LPC Meeting for their review. This project is also scheduled for a future ZAB hearing pending the conclusion of any Use Permit and CEQA review.

The project is before the Design Review Committee this month as a Preview.

II. Background

This project proposes a 159,143 square-foot, three-story research and development building and a surface parking lot containing 75 parking spaces, referred to as the West Block (Phase III). The project includes:

- An approximately 6,000 square-foot outdoor plaza at northeast portion of the property.
- Six-foot tall security fence and retaining walls at the northeast, south and west portions of the property.
- Four terraces totaling 2,592 square feet on the second and third floors facing Fourth Street and Bancroft Way.

- Vehicular access through two aisles on Bancroft Way: one for access to two loading spaces and one for access to a surface parking lot with capacity for 75 parking spaces and five loading spaces.
- A total of 88 bicycle parking spaces would be provided: 20 short-term spaces would be located on Fourth Street and 68 long-term spaces would be located in a bicycle room on the first floor, with controlled access from the surface parking lot at the rear of the building.
- 61 new trees would be planted (52 trees in the parking lot and nine in the plaza), as well as 11 street trees: 8 along Fourth Street and 3 along Bancroft.

III. Project Setting

A. Neighborhood/Area Description:

The 3.02-acre project site for all project phases is located in West Berkeley directly east of the City of Berkeley Aquatic Park across the Union Pacific Railroad (UPRR) tracks. Specifically, the site is bisected by Fourth Street and generally bordered by Allston Way to the north, Fifth Street to the east, Bancroft Way to the south, and the UPRR corridor to the west. Land uses in West Berkeley include manufacturing, wholesale, and industrial along with commercial and residential uses. The Project site is located south of the Fourth Street commercial and is surrounded by various industrial and commercial uses. Building heights in West Berkeley consist of one- and two-story residential uses, generally east of Fifth Street, and up to six-story mixed-use residential buildings adjacent to the University Avenue overpass.

B. Site Conditions:

The project site comprises seven parcels and nine existing buildings across two contiguous blocks, referred to as the “East Block” and the “West Block”. The project in this application is located in the “West Block” and consists of four parcels containing six existing vacant one-story buildings totaling 80,234 square feet with addresses at 701, 703, 705, 705A, and 747 Bancroft Way (light manufacturing, warehouse, warehouse-based non-store retail) and 2220 Fourth Street (warehouse-based non-store retail), as well as a surface parking lot containing 26 spaces.

Figure 1: Vicinity Map



Table 1: Land Use Information

Location		Existing Use	Zoning District	General Plan Designation
Subject Property		Warehouse-based non-store retail (vacant), warehouse and light manufacturing (vacant)	MU-LI	M – Manufacturing
Surrounding Properties	North	Light manufacturing (vacant)		
	South	Light manufacturing		
	East			
West	Research and Development (600 Addison campus, recently approved)			

Table 2: Development Standards

		Existing	Proposed Total	Permitted/ Required
Phase III (West Block): 747 (787) Bancroft Way				
BMC Sections 23E.80.070-080				
Number of Parcels		4	1	N/A
Lot Area (sq. ft.)		96,966	No change	N/A
Gross Floor Area (sq. ft.)		80,234	159,143	193,932 max
Floor Area Ratio (FAR)		0.83	1.64	2 max
Building Height	Average	Varies – 16' - 32'	45'	45' max
	Maximum	Varies – 16' - 35'	45'	N/A
	Stories	1	3	3 max
Building Setbacks	Front (Fourth)	0'	No change	N/A
	Rear	0'	33'-8"	N/A
	Left Side (Bancroft)	0'	0'	N/A
	Right Side	0'	21'-10"	N/A
Parking	Automobile	26	294 ²	274 min ³
	Bicycle	0	68/20 (long-term/short-term)	80 min (1:2,000 square feet)
	Loading	0	7	7 min (1:10,000 initial square feet and 1:25,000 square feet for additional)

Notes:

1 See Table 6, Summary of Proposed and Required Parking Spaces, by Phase

2 See Table 6. 75 spaces provided on-site in surface parking lot, 219 spaces provided off-site at 2213 Fourth garage.

3 Required Parking (R&D) = 2 spaces/1,000 sq.ft.; 125,796 sq.ft./500 sq.ft = 252 spaces required.

Required Parking (Light Manufacturing) = 2 spaces/1,500 sq.ft.; 33,347 sq.ft./1,500 sq.ft. = 22 spaces required. Total required spaces: 252 + 22 = 274.

IV. Project Description

A. Requested Use Permits

- Variance pursuant to BMC Section 23B.44.010 to allow replacement of protected floor area to be made available after the demolition or change of use of the space.
- Use Permit pursuant to BMC Section 23C.08.050.A to demolish main buildings used for non-residential purposes.
- Use Permit pursuant to BMC Section 23E.80.045.B, to change more than 25 percent of manufacturing and warehouse uses to research and development uses.
- Use Permit pursuant to BMC Section 23E.80.050.C to construct between 20,000 and 30,000 square feet of new floor area as Manufacturing and Wholesale Trade.
- Administrative Use Permit pursuant to BMC Section 23E.80.030 to establish more than 30,000 square feet of research and development use.
- Administrative Use Permit pursuant to BMC Section 23E.04.020.C to allow architectural elements to exceed the height limit in a non-residential district.

B. CEQA Determination

An Initial Study (IS) is being prepared to evaluate any environmental impacts of the proposed project, pursuant to CEQA.

V. Design Review Guidelines

The City's Design Guidelines are applicable for this project. Excerpts from the City-wide Design Review Guidelines are included below for your reference.

- **Setbacks:** The street façade of commercial streets should be respected, in order to create or maintain the sense of urban space.
- **Parking and Driveways:** Conflict with pedestrian circulation should be prevented by the proper location and design of auto entrances; whenever possible, parking should be behind buildings, underground, or in a central court. Shading of at least fifty percent (50%) of the surface area (as measured at 12 noon on June 21) is encouraged to reduce heat buildup and to improve visual appearance.

- **Harmony with Surroundings:** The proposed design should be in harmony with its surroundings through the coordination of such design elements as cornice lines, eaves, and setbacks with those of existing neighborhood buildings.
- **Articulation:** Street facades in general and the ground floor level in particular should include elements of pedestrian scale and three-dimensional interest.
- **Lighting:** Lighting for circulation, security, building/sign identification should be non-obtrusive, except for lighting fixtures which are themselves decorative additions to the streetscape.
- **Walls and Fences:** Large, unarticulated expanses of any particular wall material that deaden the pedestrian environment should be avoided. The use of clear windows for ground floor retail projects is encouraged. Walls designed to allow sitting areas for pedestrian or space for landscaping and artwork are encouraged, especially in areas of heavy pedestrian use. Landscaping and/or art work should be maximized if large expanses of wall must be left devoid of openings.
- **Landscape and Open Space:** Sidewalk areas should include landscaping that is coordinated with the neighborhood design.
- **Public Open Space Areas:** the inclusion of public open spaces is encouraged as a means of providing places for people to come together for community interaction and enlivening the pedestrian environment.
- **Pedestrian Paths:** Pedestrian paths and arcades interior to the block which joins different parts of buildings as well as different streets are encouraged.
- **Building Entrances:** Entrance points should be clearly defined and easily identifiable by pedestrians by appropriate locations and by elements such as awnings, signage, artwork or changes in paving material to define the entry point.

The West Berkeley Plan is also applicable to this project. Below are several relevant goals from that plan:

- Physical Form Goal 5: Development on major sites of 1 acre or more should be both internally cohesive and sensitively designed on the site's publicly used edges.
- Open Space Goal 9: Provide an accessible, aesthetically-pleasing network of green spaces and corridors - that is functional for varied types of users--to visually and physically link parks, creeks, and shoreline to residential and commercial, and light industrial areas.

The West Berkeley Plan can be found on the City's website at the link below:

[https://www.cityofberkeley.info/Planning_and_Development/Redevelopment_Agency/West_Berkeley_Plan_\(The\).aspx](https://www.cityofberkeley.info/Planning_and_Development/Redevelopment_Agency/West_Berkeley_Plan_(The).aspx)

VI. Issues and Analysis

A. Design Review Issues:

Neighborhood Context The project site is located in West Berkeley and is surrounded by the UPRR tracks to the west, Fourth Street to the east, and Bancroft Way to the south. The 96,966-sq. ft. parcel is designated as Manufacturing (M) in the General Plan and Mixed Use Light Industrial (MU-LI) in the Zoning Ordinance and the West Berkeley Area Plan. The site is surrounded by manufacturing and warehouse uses.

Massing/ Building Design This three-story structure is proposed in an L-shaped configuration with frontage on both Fourth Street and Bancroft Way. A surface parking lot is proposed adjacent to the Union Pacific Railroad. The façade design features concrete columns emphasized by deep recessed windows along the East and South facades. The massing steps back at the Southwest and Northeast corners, as well as at the entrance to the main lobby. The project proposes public open space to the north of the building and public improvements along Fourth Street.

Ground Floor Design The ground floor is recessed and increases the sidewalk width for pedestrian movement and additional planting and seating. The main entry is located near the midpoint of the Fourth Street façade and opens into a lobby space that extends the depth of the building to connect to the parking lot at the interior of the lot and common open space already permitted in Phase I. Workspaces are arranged along Fourth Street with the loading dock and mechanical spaces located midblock on the Bancroft side of the project.

Setbacks The building is set back approximately 33' 8" from the rear (west) property line and approximately 21' 10" from the right (north) property line. The project meets the property line at the East and South sides.

Landscape/ Open Space A public plaza along the North edge of the property serves as open space for the project. A parklet is proposed along the Fourth Street frontage, featuring concrete benches, planting strips, and flow through planters that wrap the corner at Bancroft. Eight new street trees are proposed on Fourth street with three new street trees to be planted on Bancroft. Outdoor terraces are proposed at Northeast and Southwest corners at the second and third floor floors.

Parking Vehicular access to the surface parking lot at the interior of the site is provided at the Southwest corner of the Bancroft frontage. This lot provides 75 vehicular space. New trees are also proposed in the surface lot for tree canopy coverage, consistent with our city-wide design guideline.

Colors and Materials The project would consist of an exposed concrete structure, dark metal mullions and metal trim, and stone or precast paving.

B. Issues for Discussion:

- Neighborhood Context
- Façade Design
- Building Design/ Massing
- Open Space/ Streetscape
- Colors and Materials

VII. Recommendation

Staff recommends that the Committee discuss the issues outlined above and provide advisory comments regarding the proposed building design, its relationship with its neighborhood, and any other design-related issues for the next review.

Attachments:

1. Project Plans, received February 7, 2022
2. Applicant Statement, received December 22, 2021
3. Design Narrative, received February 7, 2022

Staff Planner: Anne Burns, aburns@ci.berkeley.ca.us, (510) 981-7410



787 BANCROFT
 787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

PROJECT TEAM
 ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
SOM
 SKIDMORE, OWINGS & MERRILL LLP
 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

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GEOTECHNICAL ENGINEER
 ROCKRIDGE GEOTECHNICAL
 270 GRAND AVE
 OAKLAND, CA 94610

SUSTAINABILITY
 ATELIER TEN
 443 TEHAMA ST. 1ST FLOOR
 SAN FRANCISCO, CA 94103

Issued For:

No. Description:	Date:
USE PERMIT APPLICATION	2021.03.25
USE PERMIT RESUBMITTAL	2021.09.13
USE PERMIT RESUBMITTAL	2021.12.17
DESIGN REVIEW COMMITTEE MEETING 1	2022.02.07

Key Plan:

Seal & Signature:
 NOT FOR CONSTRUCTION

Sheet Name:
 COVER SHEET

Project No.: 220175
 Sheet No.:
 Drawn By: Author
 Checked By: Checker
 Scale:
GP0.00.00

787 BANCROFT
 787 BANCROFT WAY, BERKELEY, CA 94710

DESIGN REVIEW COMMITTEE MEETING
 FEBRUARY 17, 2022

VICINITY MAP



LOCATION MAP



LEED CHECKLIST

787 Bancroft Way, Berkeley
LEED v4 BD+C: Core & Shell
Updated: 11/5/2021

Category	Points	Target	Status
64 Projected Points	54	54	19

Category	Points	Target	Status
Integrative Process	20	20	19
Location & Transportation	3	3	1
Sustainable Sites	1	1	1
Water Efficiency	1	1	1

Category	Points	Target	Status
Energy & Atmosphere	3	3	1
Materials & Resources	2	2	1
Indoor Environmental Quality	1	1	1
Innovation	1	1	1
Regional Priority	1	1	1

PARKING AND LOADING REQUIREMENTS

REQUIRED VEHICLE PARKING (R&D) = 2 SPACES / 1,000 SF (BERKELEY MUNICIPAL CODE SEC 23E.80.080)
R&D AREA = 125,796 GSF / 500 SF = 252 PARKING SPACES REQUIRED
REQUIRED VEHICLE PARKING (LIGHT MAN) = 2 SPACES / 1,500 SF (BERKELEY MUNICIPAL CODE SEC 23E.80.080)
LM AREA = 33,347 GSF / 1,500 SF = 22 PARKING SPACES REQUIRED
TOTAL AREA = 252 + 22 = 274 PARKING SPACES REQUIRED

REQUIRED LOADING SPACES = 1 SPACE / 10,000 SF + 1 SPACE / ADD 25,500 SF (BMC 23E.80.080)
INITIAL AREA = 159,143 = 10,000 = 1 LOADING SPACE
REMAINING AREA = 159,143 - 10,000 = 149,143 SF / 25,500 SF = 6 LOADING SPACES
TOTAL AREA = 6 + 1 = 7 LOADING SPACES REQUIRED

REQUIRED BICYCLE PARKING = 1 SPACE / 2,000 SF (BERKELEY MUNICIPAL CODE 23E.80.080/23E.28.070)
LONG-TERM = (LIGHT MANUFACTURING = 1 SPACE / 30,000 SF) + (OFFICE / R&D = 1 SPACE / 2,000 SF)
LM AREA = 33,347 SF / 30,000 SF = 1 LONG TERM BICYCLE PARKING
OFFICE / R&D = 125,796 SF / 2,500 SF = 50 LONG TERM BICYCLE PARKING
SHORT-TERM = OFFICE / R&D = 1 SPACE / 10,000 SF
OFFICE / R&D = 125,796 SF / 10,000 SF = 13 LONG TERM BICYCLE PARKING
TOTAL PARKING = 57 LONG-TERM AND 13 SHORT-TERM BICYCLE PARKING SPACES REQUIRED

PARKING AND LOADING SUMMARY

Category	Provided	Required
VEHICULAR PARKING		
STANDARD PARKING:	71	
ADA PARKING:	4	
VAN ACCESSIBLE PARKING:	1	
ON-SITE PARKING SUBTOTAL:	76	
OFF-SITE GARAGE PARKING:	219	198
TOTAL VEHICLE PARKING:	295	274
TOTAL EV CHARGING SPACES	30	30
ON-SITE:	14	
OFF-SITE:	16	
TOTAL EV CAPABLE SPACES	118	118
ON-SITE:	14	
OFF-SITE:	104	
CLEAN AIR VEHICLE SPACES	29	29
ON-SITE:	11	
OFF-SITE:	18	
BICYCLE PARKING		
SHORT TERM (4TH ST)	20	13
LONG TERM ON SITE	68	51
LONG TERM GARAGE	0	
TOTAL BICYCLE PARKING:	88	80
BUILDING LOADING		
TRUCK BAYS	2	
LOADING SPACES	3	
TOTAL LOADING	5	7

PROJECT INFORMATION

NAME: 787 BANCROFT
ADDRESS: 787 BANCROFT WAY, BERKELEY, CA 94710
AREA OF WORK: 96,966 SF
NUMBER OF STORIES: 3
BUILDING HEIGHT: 45'-0"
SCOPE OF WORK: THE PROJECT PROPOSES A THREE STORY LAB / OFFICE BUILDING AND SURFACE PARKING. EXISTING STRUCTURES ON SITE WILL BE DEMOLISHED OR REPURPOSED.

CODE COMPLIANCE AND ANALYSIS

BUILDING CODE: 2019 CALIFORNIA BUILDING CODE
ELECTRICAL CODE: 2019 CALIFORNIA ELECTRICAL CODE
MECHANICAL CODE: 2019 CALIFORNIA MECHANICAL CODE
PLUMBING CODE: 2019 CALIFORNIA PLUMBING CODE
ENERGY CODE: 2019 CALIFORNIA ENERGY CODE
SUSTAINABILITY CODE: 2019 CALIFORNIA GREEN BUILDING STANDARDS
FIRE CODE: 2019 CALIFORNIA FIRE CODE
ZONING: MU-LI
LOT NUMBERS: 056-1957-003-01, 056-1957-002-04, 056-1957-007-01

OCCUPANCY TYPE: A-3 (ASSEMBLY), B (OFFICE), L (LAB), S-1 (HAZMAT STORAGE), S-2 (STORAGE)
OCCUPANCY SEPARATIONS: NON-SEPARATED, MIXED USE. Not required for "non-separated" occupancies, per BC 508.3, except for those listed in CBC 508.3.3 Exception 1 (which include 1 hour for "L" Laboratory occupancies, per CBC Table 508.4)
TYPE OF CONSTRUCTION: TYPE 1A (PER CBC 603)
SPRINKLERED: FULLY SPRINKLERED WITH AUTOMATIC SPRINKLER SYSTEM
FIRE ALARM: YES
FIRE EXTINGUISHERS: TYPE 2-A FIRE EXTINGUISHERS EVERY 75' OR 11,250 SF MAX
FIRE SERVICE ACCESS ELEVATORS: NOT REQUIRED (PER CBC 406.3.1, HIGHEST OCCUPIED FLOOR <120')

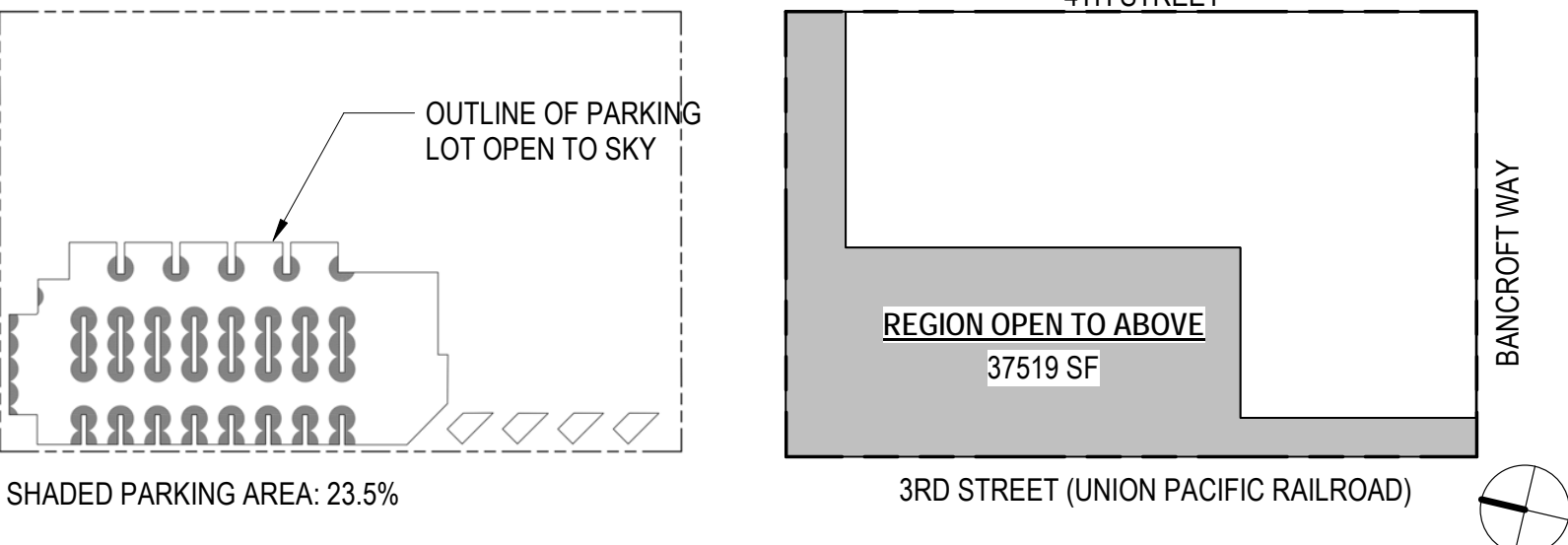
PER CBC TABLE 601 FIRE-RESISTANCE RATING REQUIREMENTS FOR BUILDING ELEMENTS (HOURS) - TYPE 1A CONSTRUCTION

BUILDING ELEMENT	REQUIRED RATING
PRIMARY STRUCTURAL FRAME	3 HRS
BEARING WALLS EXTERIOR	3 HRS
BEARING WALLS INTERIOR	3 HRS
NONBEARING WALLS & PARTITIONS EXTERIOR	0 HR (OR AS REQUIRED PER CBC TABLE 602)
NONBEARING WALLS & PARTITIONS INTERIOR	0 HR (OR 1 HR BETWEEN B & L OCCUPANCIES, AND ADJACENT L OCCUPANCY SUITES, PER CBC 453.3.1)
FLOOR CONSTRUCTION AND ASSOCIATED SECONDARY MEMBERS:	2 HRS
ROOF CONSTRUCTION AND ASSOCIATED SECONDARY MEMBERS:	1 1/2 HRS
SHAFT ENCLOSURES:	2 HRS
STAIR ENCLOSURES:	2 HRS

AREA SUMMARY

Level	R&D	LM	Service Towards LM	Service Towards R&D	Total Service	Total
Roof						0
Level 03	56,310					56,310
Level 02	56,310					56,310
Level 01	6,169	30,191	3,156	7,007	10,163	46,523
Subtotal	125,796	33,347				159,143

OPEN SPACE SUMMARY



SHEET INDEX

SHEET #	SHEET NAME	2021.03.25 - USE PERMIT APPLICATION	2021.09.13 - USE PERMIT RESUBMITTAL	2021.12.17 - USE PERMIT RESUBMITTAL	2022.02.07 - DESIGN REVIEW COMMITTEE MEETING 1
GP0.00.00	COVER SHEET				
GP0.01.00	PROJECT INFORMATION AND SHEET INDEX				
GP0.01.00A	LAND USE AREAS				
GP0.01.01	PHOTO SIMULATIONS AND VISUALIZATION				
GP0.01.02	PHOTO SIMULATIONS AND VISUALIZATION				
GP0.01.03	PHOTO SIMULATIONS AND VISUALIZATION				
GP0.01.04	MATERIAL PALETTE				
GP0.02.01	EXISTING PARCEL DEMOLITION PLAN				
GP0.02.04	SITE PHOTOGRAPHS OF BUILDINGS TO BE DEMOLISHED				
GP0.03.00	AVERAGE ELEVATION CALCULATION				
GP0.04.00	BICYCLE PARKING SPECIFICATIONS				
11	CIVIL				
CP1.00	TOPOGRAPHY SURVEY				
CP2.00	CONCEPTUAL DEMOLITION PLAN				
CP3.00	CONCEPTUAL GRADING PLAN				
CP4.00	CONCEPTUAL STORMWATER MANAGEMENT PLAN				
CP5.00	OVERALL SITE PLAN				
5	ARCHITECTURAL				
AP1.01.00	LAB MASTERPLAN				
AP1.01.01	OVERALL SITE PLAN				
AP2.01.00	OVERALL FLOOR PLAN - LEVEL 01				
AP2.02.00	OVERALL FLOOR PLAN - LEVEL 02				
AP2.03.00	OVERALL FLOOR PLAN - LEVEL 03				
AP2.04.00	OVERALL FLOOR PLAN - MECHANICAL PENTHOUSE				
AP2.05.00	OVERALL FLOOR PLAN - ROOF				
AP5.00.01	EXISTING ELEVATIONS				
AP5.00.02	SITE ELEVATIONS				
AP5.00.03	STREET STRIP ELEVATIONS				
AP5.01.01	EAST AND SOUTH ELEVATIONS				
AP5.01.02	WEST AND NORTH ELEVATIONS				
AP5.02.01	SECTIONS				
AP5.03.01	EXTERIOR WALL DETAILS				
14	LANDSCAPE				
LP1.00.00	LEGEND/SCHEDULE				
LP2.00.00	LAB LANDSCAPE MASTER PLAN				
LP2.00.01	LANDSCAPE PLAN				
LP2.00.11	ENLARGEMENT PLAN				
LP4.00.00	IRRIGATION CONCEPT PLAN				
LP5.00.00	PRECEDENT IMAGES				
LP5.01.00	PLANTING PLAN - GROUND LEVEL TREE				
LP5.01.01	PLANTING PLAN - GROUND LEVEL SHRUB				
LP5.03.00	SHADED PARKING COVERAGE PLAN				
LP8.00.01	DETAILS				
10	TOTAL SHEETS: 40				

787 BANCROFT
787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
101 CALIFORNIA STREET, SUITE 800
SAN FRANCISCO, CA 94111

PROJECT TEAM
ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
SOM
SKIDMORE, OWINGS & MERRILL LLP
ONE MARITIME PLAZA
SAN FRANCISCO, CA 94111

BUILDER
LUSARDI CONSTRUCTION COMPANY
6376 CLARK AVE
DUBLIN, CA 94568

CIVIL ENGINEER
WARE MALCOMB
4633 CHABOT DRIVE #300
PLEASANTON, CA 94588

MEP ENGINEER
MEYERS+ ENGINEERS
99 BATTERY STREET, SUITE 500
SAN FRANCISCO, CA 94111

GEOTECHNICAL ENGINEER
ROCKRIDGE GEOTECHNICAL
270 GRAND AVE
OAKLAND, CA 94610

SUSTAINABILITY
ATELIER TEN
443 TEHAMA ST. 1ST FLOOR
SAN FRANCISCO, CA 94103

Issued For:

No.	Description	Date
	USE PERMIT APPLICATION	2021.03.25
	USE PERMIT RESUBMITTAL	2021.09.13
	USE PERMIT RESUBMITTAL	2021.12.17
	DESIGN REVIEW COMMITTEE MEETING 1	2022.02.07

Key Plan:

SHEET #	SHEET NAME	2021.03.25	2021.09.13	2021.12.17	2022.02.07
10	TOTAL SHEETS: 40				

Seal & Signature:

NOT FOR CONSTRUCTION


PROJECT INFORMATION AND SHEET INDEX

Project No.: 220175
Sheet No.: GP0.01.00
Scale: 1" = 100'-0"

787 BANCROFT

787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
 **STEELWAVE**
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

PROJECT TEAM
 ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
 **SOM**
 SKIDMORE, OWINGS & MERRILL LLP
 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

BUILDER
 LUSARDI CONSTRUCTION COMPANY
 6376 CLARK AVE
 DUBLIN, CA 94568

CIVIL ENGINEER
 WARE MALCOMB
 4683 CHABOT DRIVE #300
 PLEASANTON, CA 94588

MEP ENGINEER
 MEYERS+ ENGINEERS
 98 BATTERY STREET, SUITE 500
 SAN FRANCISCO, CA 94111

GEOTECHNICAL ENGINEER
 ROCKRIDGE GEOTECHNICAL
 270 GRAND AVE
 OAKLAND, CA 94610

SUSTAINABILITY
 ATELIER TEN
 443 TEHAMA ST. 1ST FLOOR
 SAN FRANCISCO, CA 94103

Issued For:

No. Description:	Date:
USE PERMIT APPLICATION	2021.03.25
USE PERMIT RESUBMITTAL	2021.09.13
100% DESIGN DEVELOPMENT	2021.11.05
USE PERMIT RESUBMITTAL	2021.12.17
DESIGN REVIEW COMMITTEE MEETING 1	2022.02.07

Key Plan:

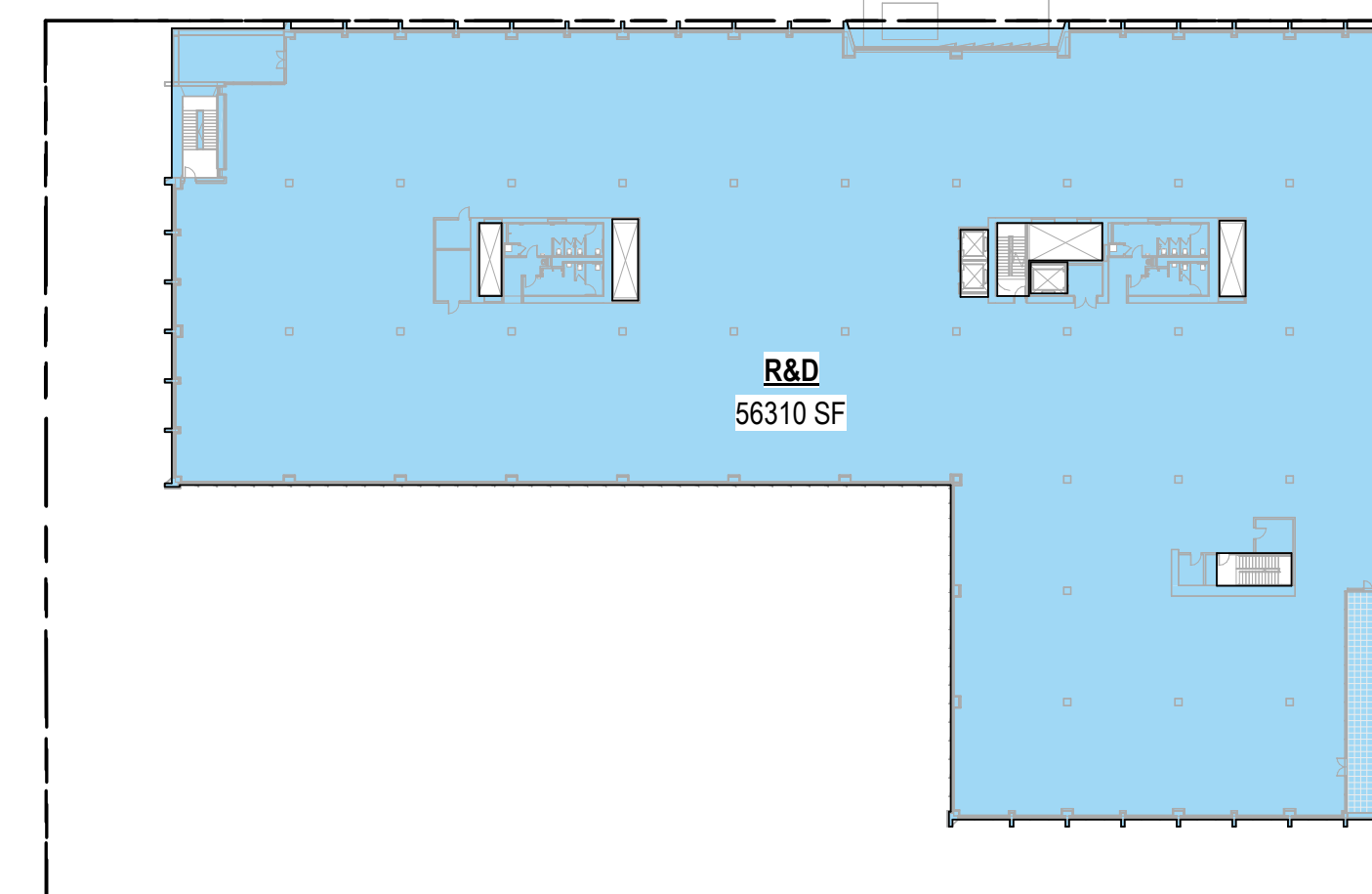
Seal & Signature:

NOT FOR CONSTRUCTION

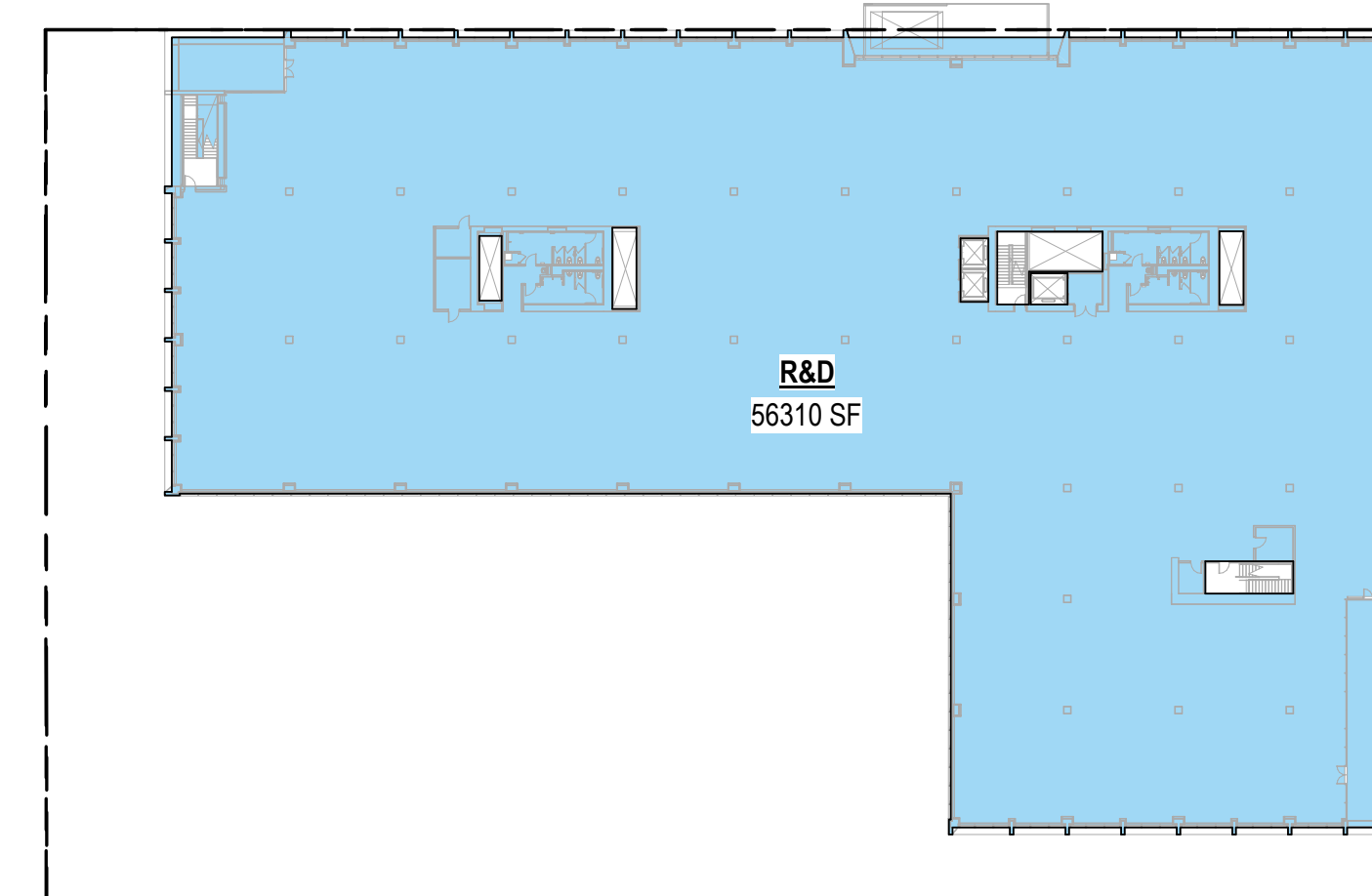
Sheet Name:

LAND USE AREAS

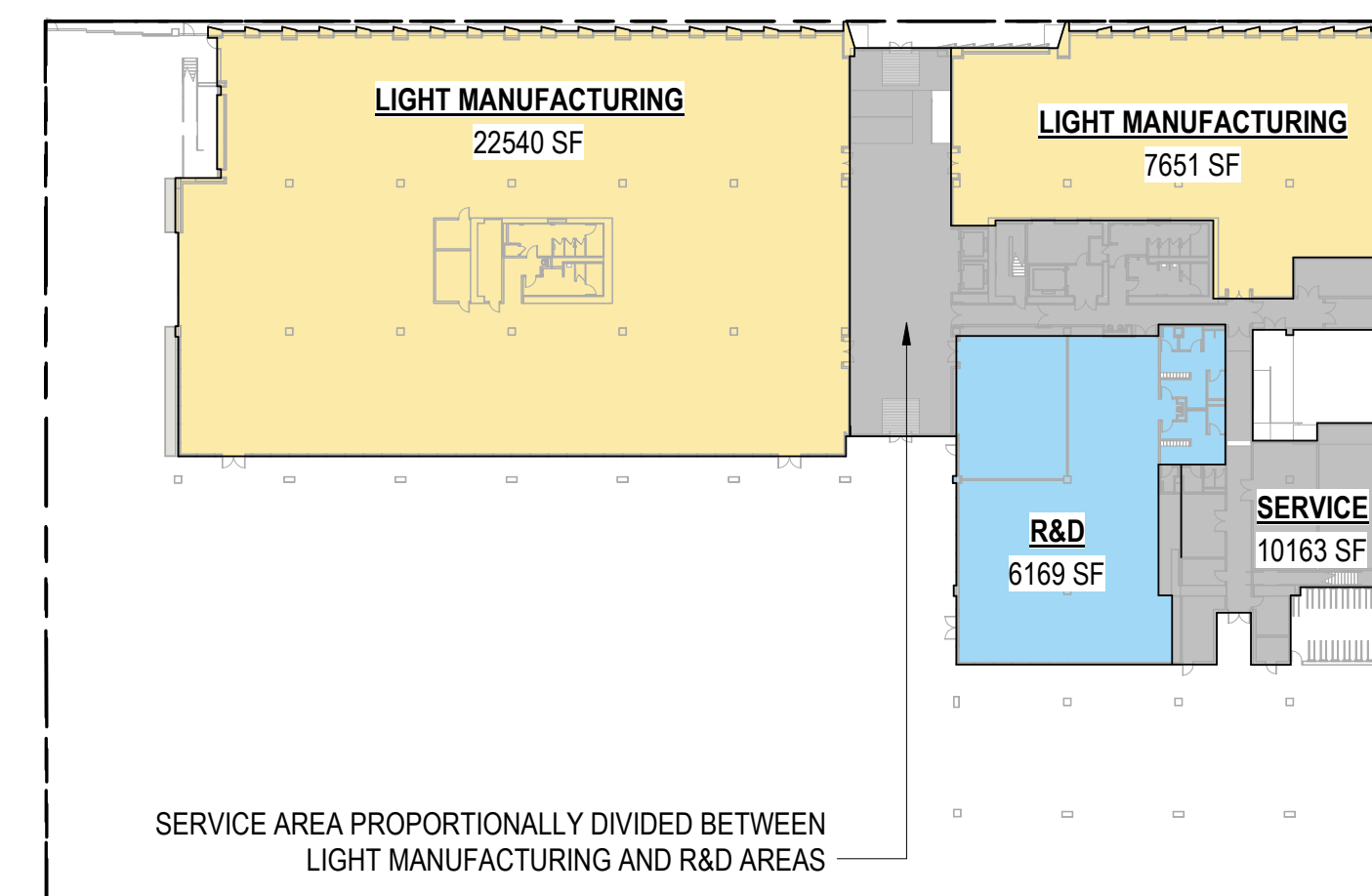
Project No.: 220175
 Sheet No.: GP0.01.00A
 Drawn By: Author
 Checked By: Checker
 Scale: As indicated



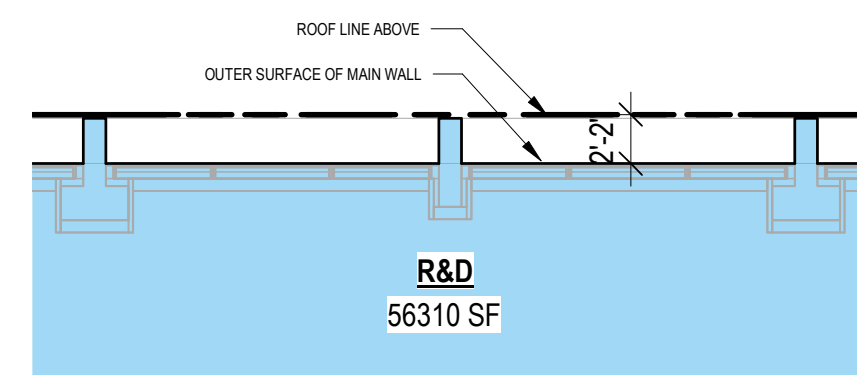
LEVEL 03 LAND USE 3
SCALE: 1" = 50'-0"



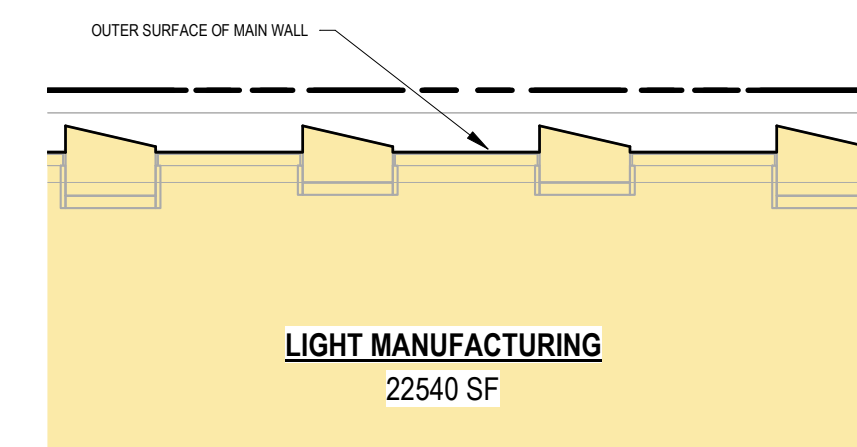
LEVEL 02 LAND USE 2
SCALE: 1" = 50'-0"



LEVEL 01 LAND USE 1
SCALE: 1" = 50'-0"



LEVEL 02-03 AREA BOUNDARY DETAIL 5
SCALE: 1/8" = 1'-0"



LEVEL 01 AREA BOUNDARY DETAIL 4
SCALE: 1/8" = 1'-0"

Land Use Area Summary

ALL LAND USE AREAS ARE CALCULATED AS GROSS FLOOR AREA ACCORDING TO BMC SECTION 23F.04.010

Level	R&D	LM	Service Towards LM	Service Towards R&D	Total Service	Total
Roof						0
Level 03	56,310					56,310
Level 02	56,310					56,310
Level 01	6,169	30,191	3,156	7,007	10,163	46,523
Subtotal	125,796	33,347				159,143
Area Counted Toward FAR:						159,143

GENERAL NOTE:

- LOCATION OF LIGHT MANUFACTURING AREA WILL BE PLACED AS NEEDED FOR INITIAL LEASE-UP
- REQUIRED PROTECTED AREA WILL BE MAINTAINED
- ALL LAND USE AREAS ARE CALCULATED AS GROSS FLOOR AREA ACCORDING TO BMC SECTION 23F.04.010
- SEE AREA CALCULATION METHODOLOGY DIAGRAMS FOR CALCULATION METHOD



VIEW FROM 4TH AND BANCROFT: EXISTING



VIEW FROM 4TH AND BANCROFT: PROPOSED



VIEW FROM 4TH STREET: EXISTING



VIEW FROM 4TH STREET: PROPOSED



787 BANCROFT

787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
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 ONE MARITIME PLAZA
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 MEYERS+ ENGINEERS
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 ROCKRIDGE GEOTECHNICAL
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Key Plan:

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Project No.: 220175	Sheet No.:
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VIEW FROM RAILROAD CROSSING: EXISTING



VIEW FROM RAILROAD CROSSING: PROPOSED



VIEW FROM RAILROAD CROSSING: EXISTING



VIEW FROM WEST: PROPOSED



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VIEW FROM BANCROFT WAY: EXISTING



VIEW FROM BANCROFT WAY: PROPOSED



VIEW FROM 4TH STREET: EXISTING



VIEW FROM 4TH STREET: PROPOSED



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MATERIALITY AND CHARACTER

The construction and materiality building is inspired by the functionality and playfulness of the industrial buildings in the neighborhood. There is a clear expression of the structural frame and a play in rhythm of the cast in place concrete that is also the primary building facade.



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MATERIAL PALETTE

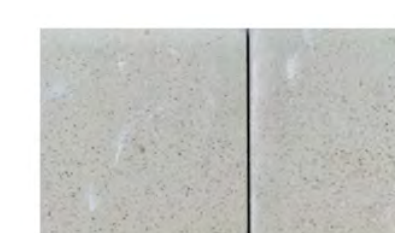
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Scale:			



VIEW OF 4TH STREET ENTRANCE



1 Exposed Concrete Structure



4 Stone or Precast Paving



2 Dark Metal Mullions and Metal Trim



5 Bay Friendly Landscaping



3 Glass



VIEW OF 4TH STREET PLAZA

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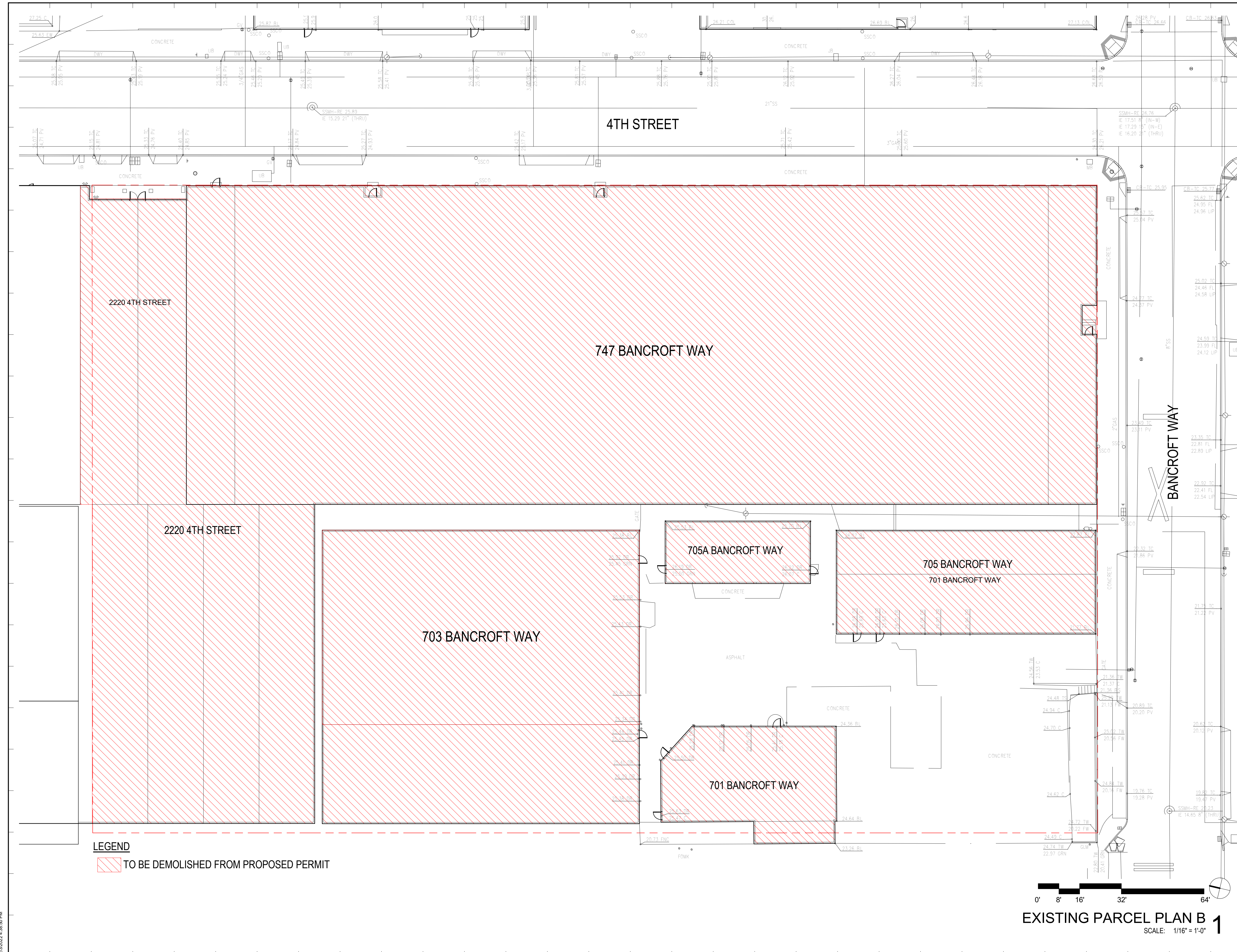
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EXISTING PARCEL DEMOLITION PLAN

Project No.: 220175
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GP0.02.01



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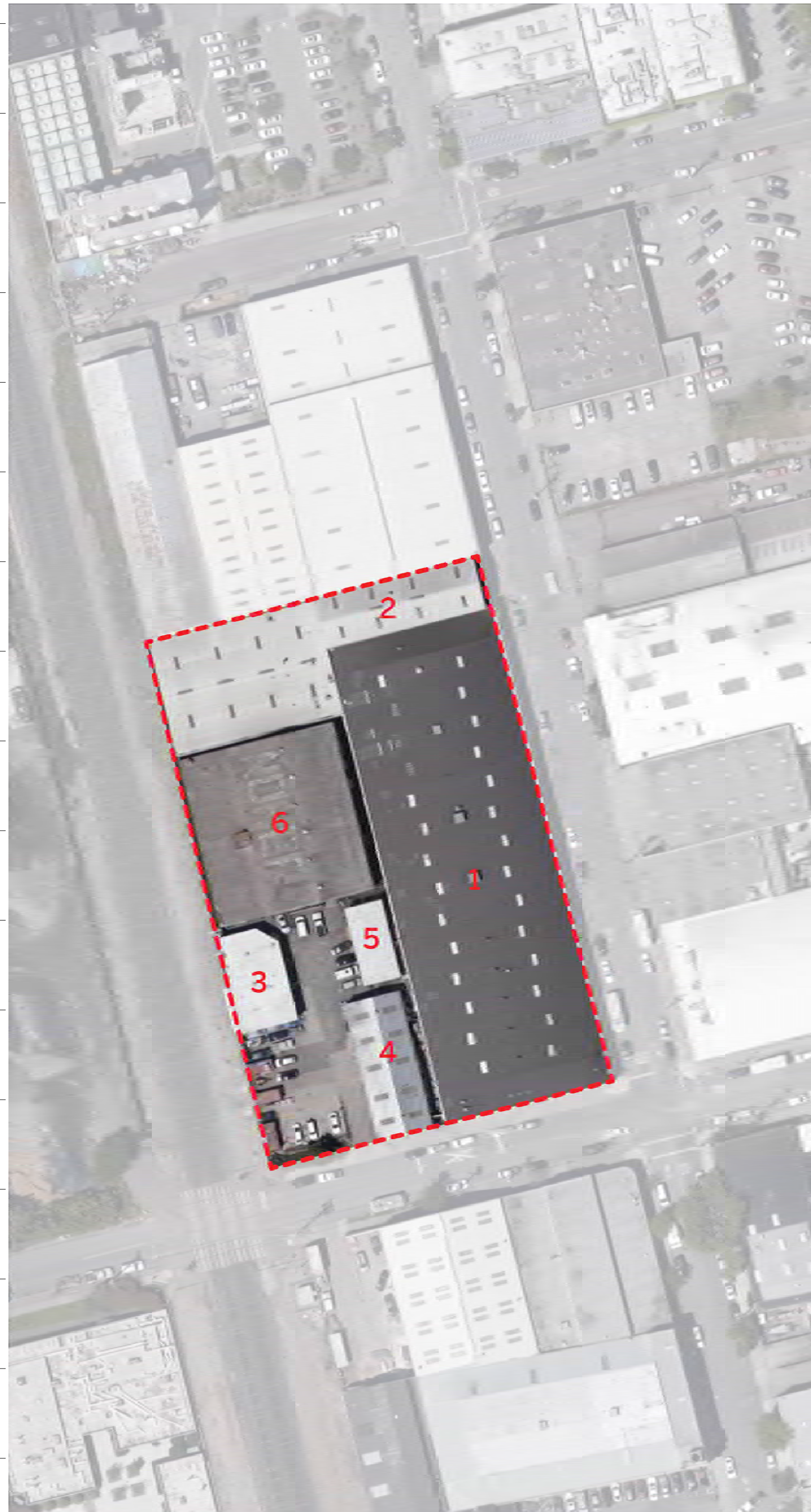
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SITE PHOTOGRAPHS OF BUILDINGS TO BE DEMOLISHED

Project No.: 220175	Sheet No.:
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 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

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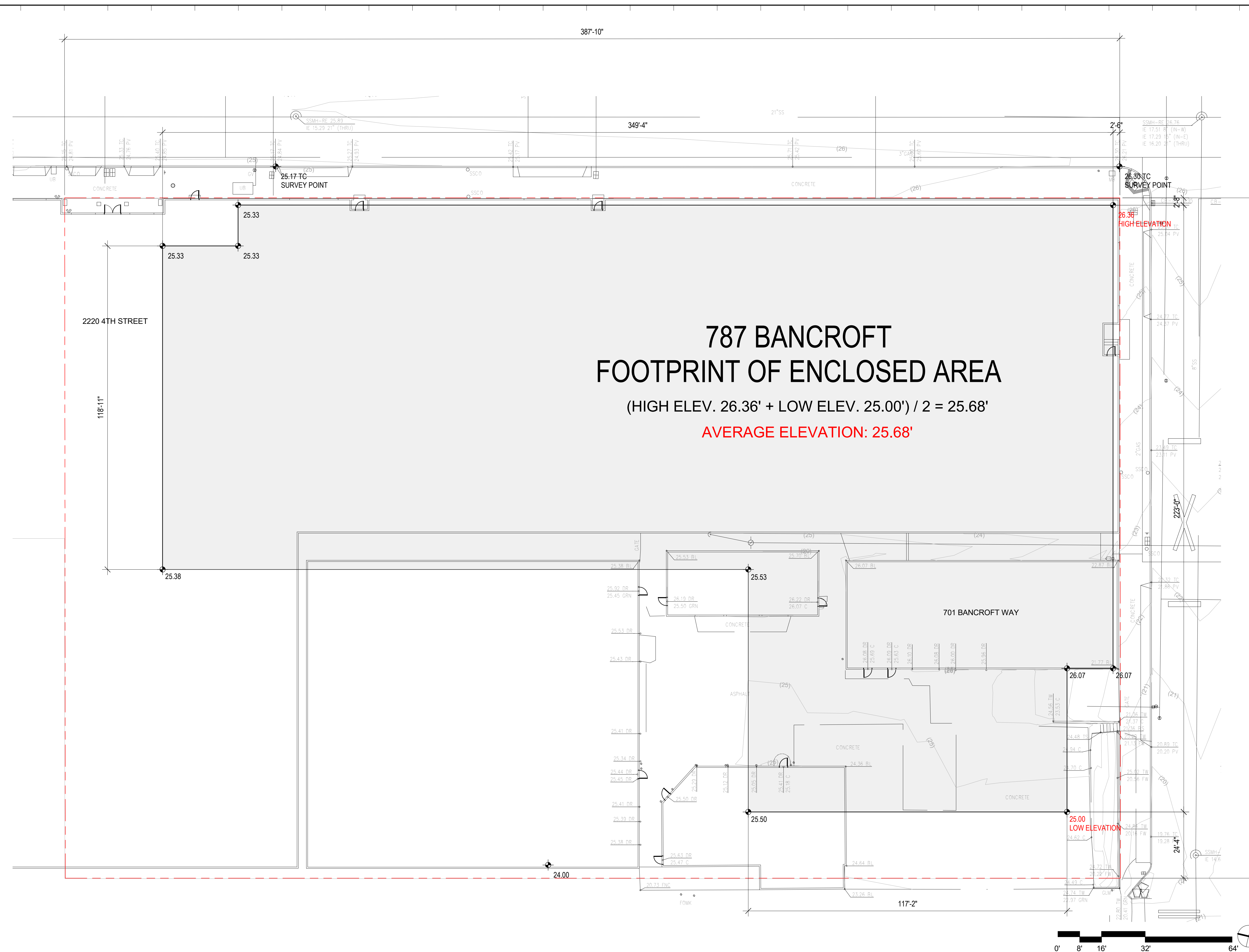
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**AVERAGE
 ELEVATION
 CALCULATION**

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Scale: 1/16" = 1'-0"	



**787 BANCROFT
 FOOTPRINT OF ENCLOSED AREA**
 (HIGH ELEV. 26.36' + LOW ELEV. 25.00') / 2 = 25.68'
AVERAGE ELEVATION: 25.68'

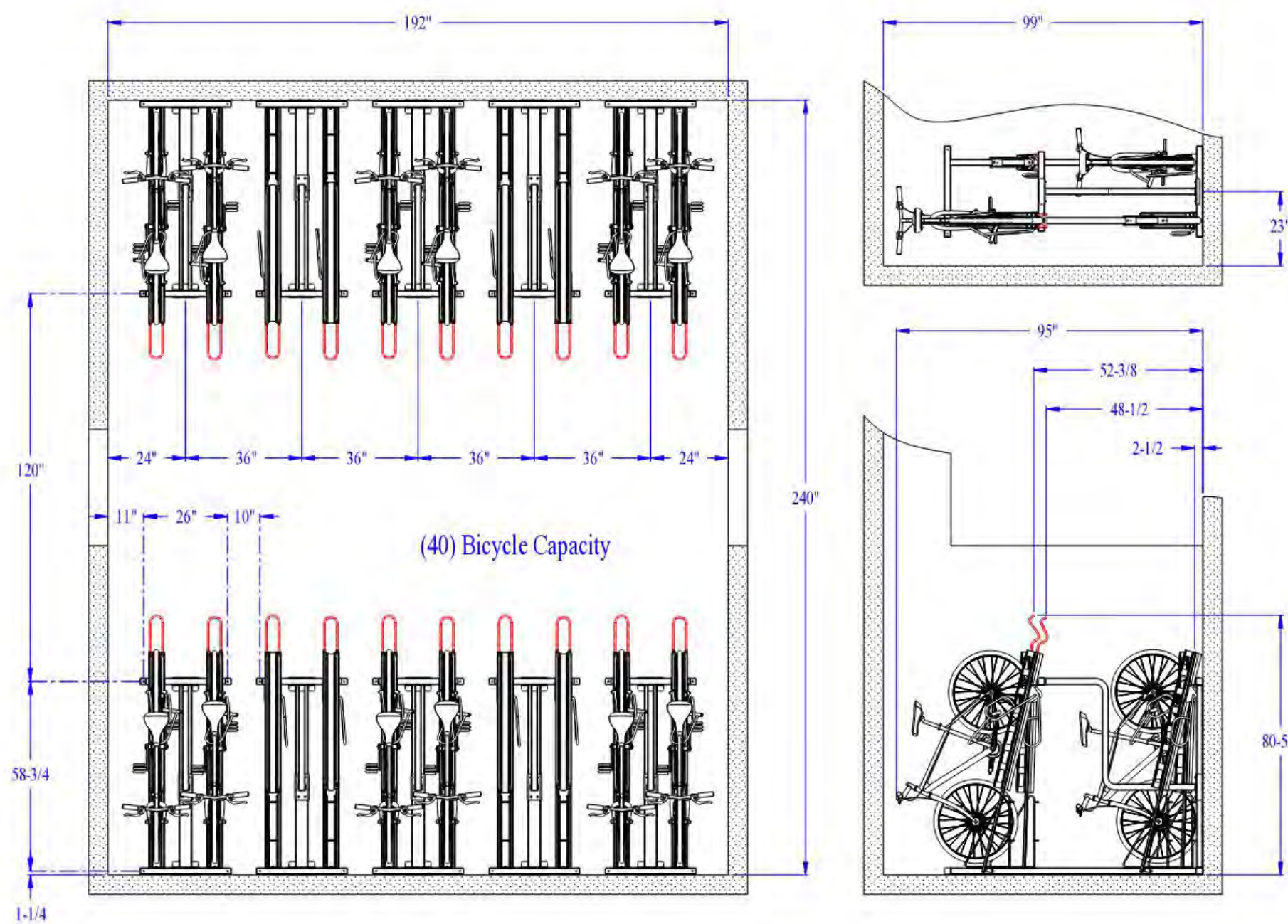
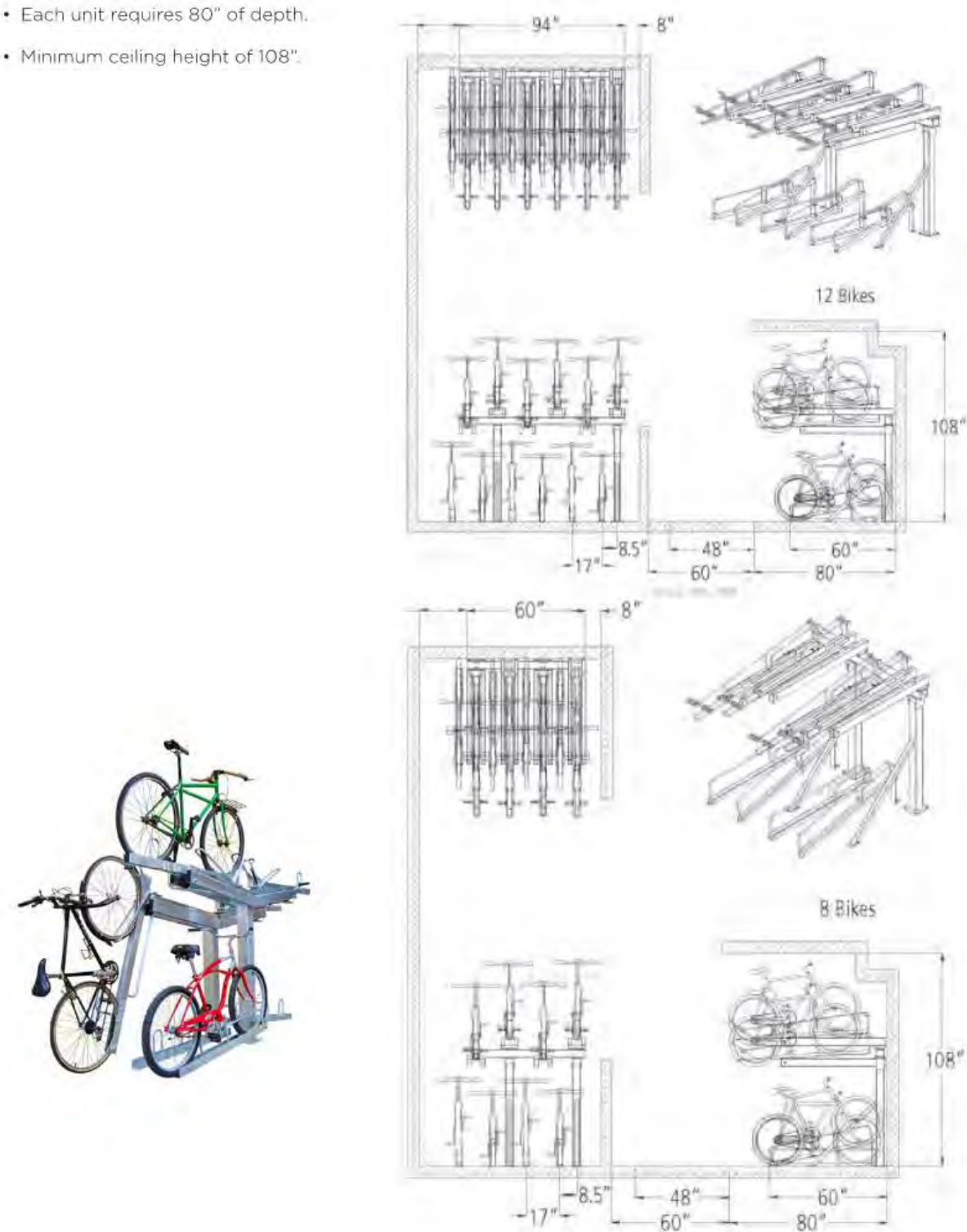
BUILDING FOOTPRINT 1
 SCALE: 1/16" = 1'-0"

Appendix F: Bike Parking

HORIZONTAL, TWO-TIERED PARKING RACK

Lift-assist top tray, modular/stacking

- Recommended access aisle of 60", minimum access aisle of 48".
- Each unit requires 80" of depth.
- Minimum ceiling height of 108".



9/10/21, 3:53 PM Double Tier Bike Racks - 2-Tier Quad Hi-Density Bike Rack - CycleSafe



Home | Secure Bike Parking | Bike Racks | Quad Hi-Density Bike Rack



Quad Hi-Density Bike Rack

Our Quad Hi-Density Bike Rack™ is a double-tier bike rack offering space-efficient, two-tier bike parking. Easy to load, simply place bike into the innovative wheel guide and roll along the channel to secure the wheel and frame in place. Bicycles can be locked with a U-lock compatible pivot locking bar for added security.

Each double-tier bike rack allows two-tier bike parking to securely store four bicycles. The modular design allows for one- or two-sided capacity with multiple configurations to customize for any location. Quad Hi-Density double-tier bike racks are easy to assemble, and constructed with zinc primer TGIC polyester powder coat finish steel tubing and UV plastic wheel gutters for lasting durability.

With minimal moving parts, our double-tier bike rack design also increase access aisles, with less maintenance or liability concerns associated with movable tray racks. This innovative

https://cyclesafe.com/bike-parking/bike-racks/hi-density-bike-rack/

14

01 BICYCLE LAYOUT
(EXCERPT FROM BERKELEY BICYCLE PLAN 2017)

02 BICYCLE PARKING SPECIFICATION

NOTE: SPECIFICATION CONFORMS WITH LONG TERM BICYCLE PARKING STANDARDS FOUND IN BERKELEY BIKE PLAN 2017, PAGE 119

FIXIT



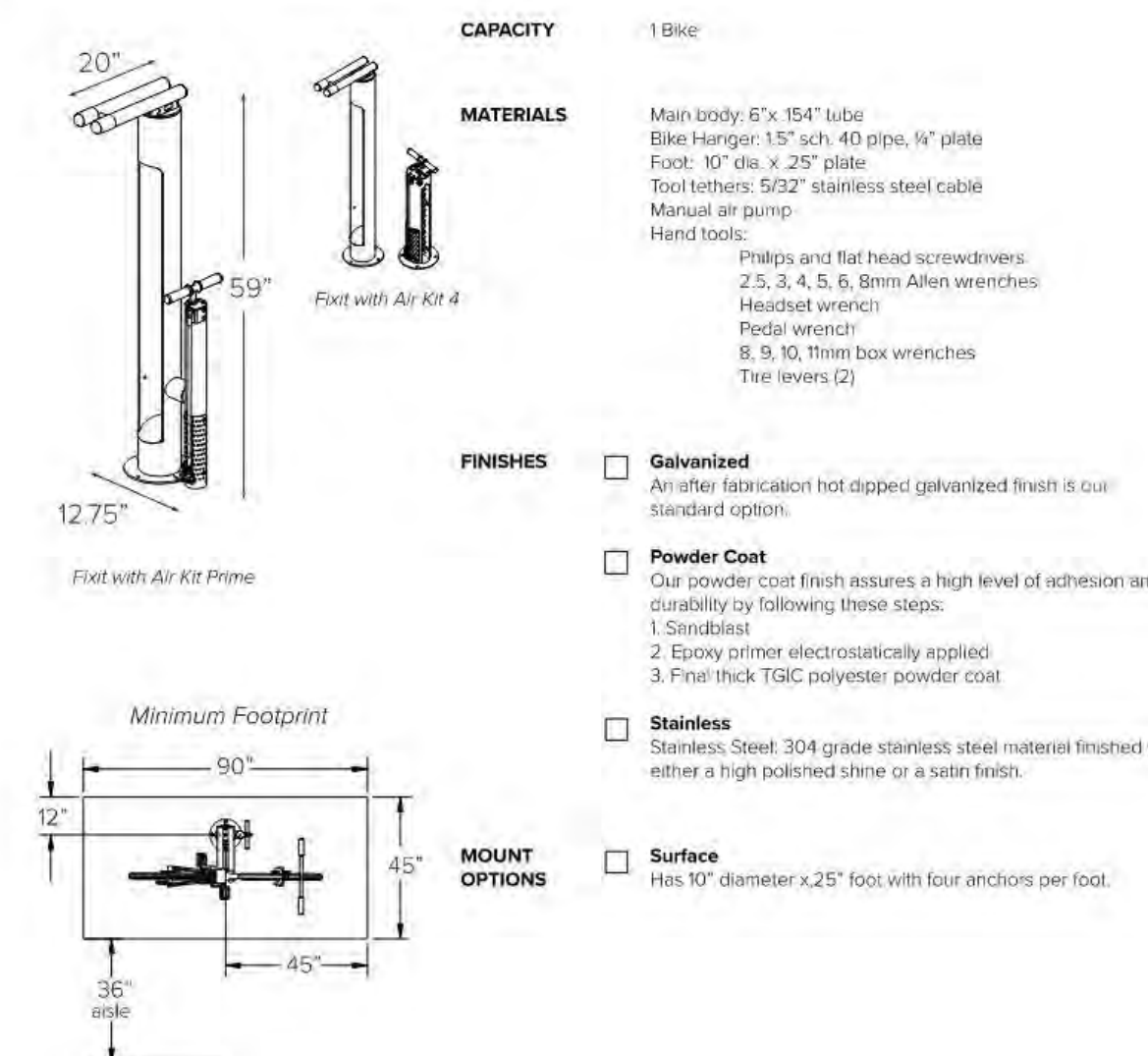
FINISH OPTIONS



04 BICYCLE REPAIR STAND SPECIFICATION

03 BICYCLE PARKING SPECIFICATION

FIXIT Submittal Sheet



787 BANCROFT

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OWNER



STEELWAVE
101 CALIFORNIA STREET, SUITE 800
SAN FRANCISCO, CA 94111

PROJECT TEAM

ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE



SKIDMORE, OWINGS & MERRILL LLP
ONE MARITIME PLAZA
SAN FRANCISCO, CA 94111

BUILDER

LUSARDI CONSTRUCTION COMPANY
6376 CLARK AVE
DUBLIN, CA 94568

CIVIL ENGINEER

WARE MALCOMB
4683 CHABOT DRIVE #300
PLEASANTON, CA 94588

MEP ENGINEER

MEYERS+ ENGINEERS
98 BATTERY STREET, SUITE 500
SAN FRANCISCO, CA 94111

GEOTECHNICAL ENGINEER

ROCKRIDGE GEOTECHNICAL
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BICYCLE PARKING SPECIFICATIONS

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TOPOGRAPHY SURVEY

Project No.:

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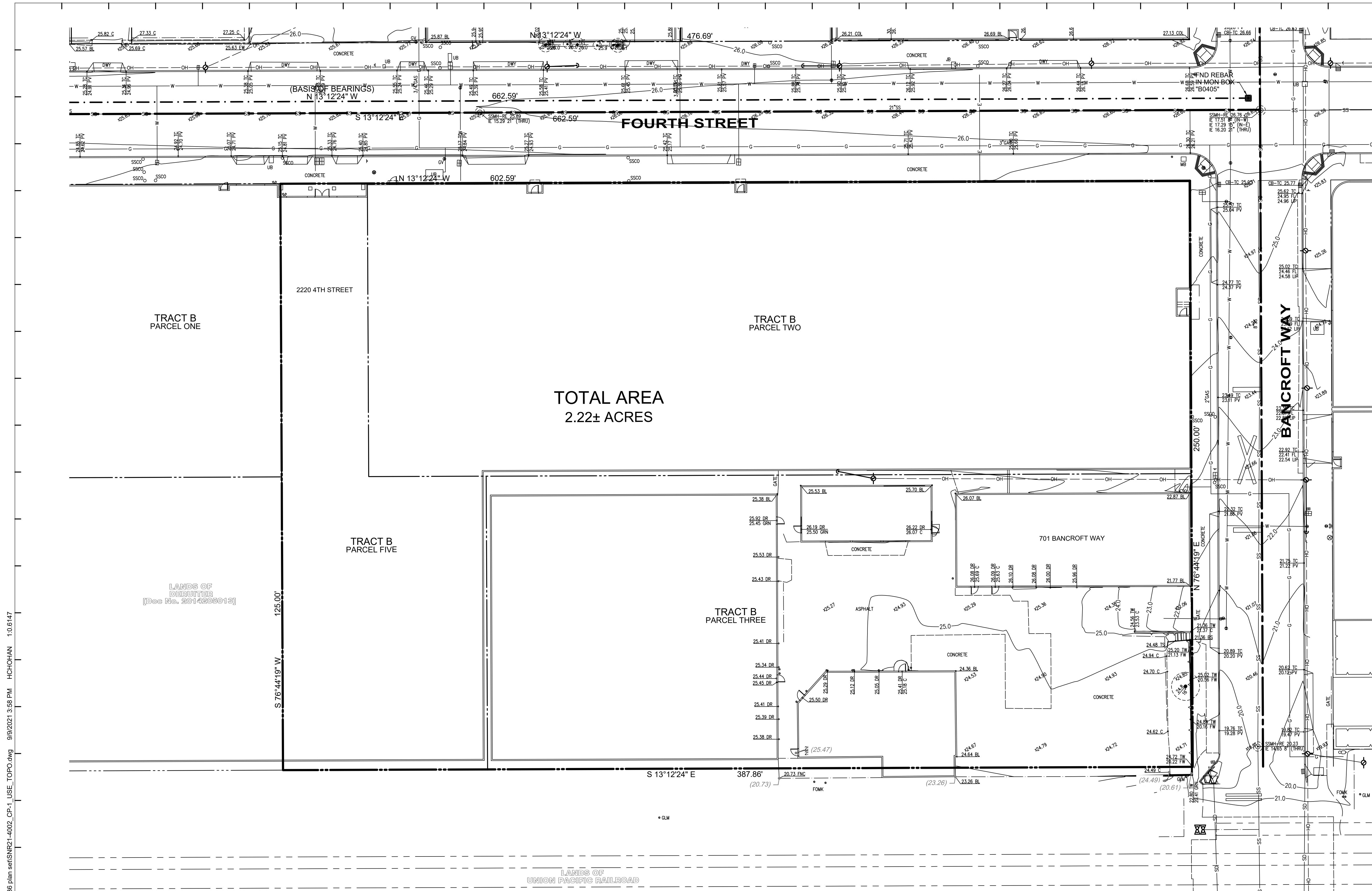
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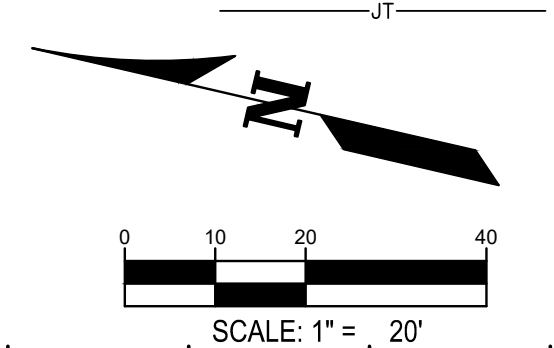


ABBREVIATIONS

AC	AIR CONDITION	GLM	GAS LINE MARKER
BL	BUILDING	GRN	GROUND
BR	BOTTOM OF RAMP	GV	GAS VALVE
BS	BOTTOM OF STEP	JB	JUNCTION BOX
CB	CATCH BASIN	LIP	LIP OF GUTTER
COL	COLUMN	MB	MAIL BOX
CTV	CABLE TELEVISION	MH	MANHOLE
DI	DROP INLET	PED	PEDESTAL
DR	DOOR	PV	PAVEMENT
DWY	DRIVEWAY	RE	RIM ELEVATION
IE	INVERT ELEVATION	SSCO	SANITARY SEWER CLEAN OUT
EB	ELECTRIC BOX	SSMH	SANITARY SEWER MANHOLE
EM	ELECTRIC METER	TC	TOP OF CURB
EP	EDGE OF PAVEMENT	TS	TOP OF STEP
EW	EDGE OF WALK	UB	UTILITY BOX
FF	FINISH FLOOR	VG	VALLEY GUTTER
FNC	FENCE	WB	WATER BOX

LEGEND

PROPERTY LINE	STORM DRAIN MANHOLE	SIGN	STORM DRAIN
ADJACENT PROPERTY LINE	SANITARY SEWER MANHOLE	TREE W/ SIZE AND ELEVATION	WATER
CENTERLINE	CLEAN OUT	SPOT ELEVATION	GAS
MONUMENT LINE	GAS METER	CONTOUR	UNDERGROUND ELECTRIC
BUILDING LINE W/ DOOR	UTILITY POLE W/ GUY WIRE	INDEX CONTOUR	OVERHEAD
BUILDING OVERHANG	VALVE	CURB	JOINT TRENCH
FOUND MONUMENT AS NOTED	CATCH BASIN / DROP INLET	CURB & GUTTER	
BOLLARD LIGHT	WATER METER	CONCRETE	
LIGHT	FIRE DEPARTMENT CONNECTION	FENCE	
STREET LIGHT	BACK FLOW PREVENTER	RETAINING WALL	
TRANSFORMER	UTILITY BOX (SIZE VARIES)	EDGE OF PAVEMENT	
FIRE HYDRANT	MONITORING WELL	SANITARY SEWER	



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**CONCEPTUAL
 DEMOLITION
 PLAN**

Project No.:

220175

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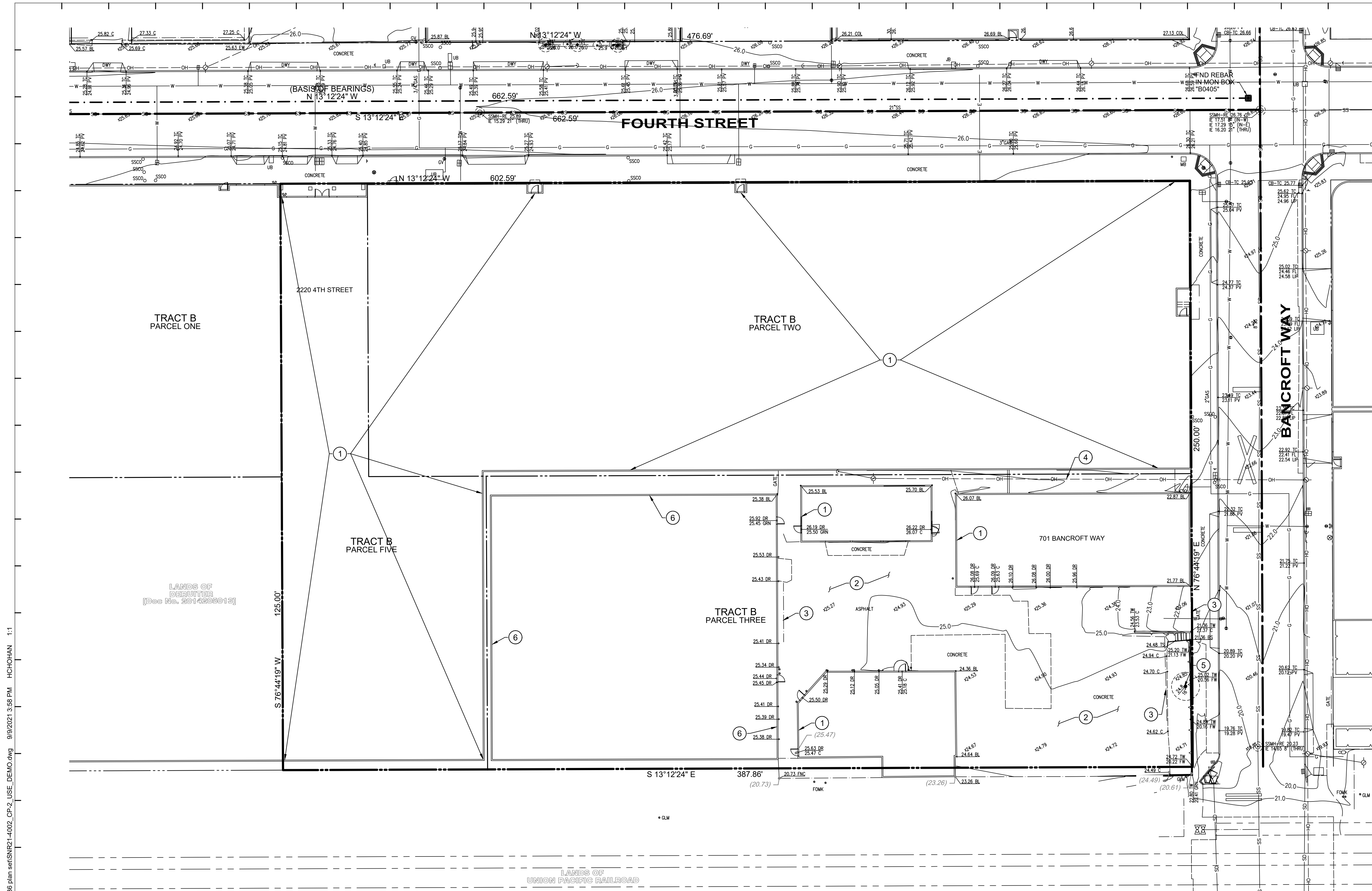
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Sheet No.:

220175

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KEYNOTES

- ① REMOVE EXISTING BUILDING
- ② REMOVE EXISTING CONCRETE AND/OR ASPHALT
- ③ REMOVE EXISTING FENCE AND/OR GATE
- ④ REMOVE EXISTING OVERHEAD UTILITIES
- ⑤ REMOVE EXISTING TREE
- ⑥ RELOCATE EXISTING STRUCTURE

LEGEND

PROPERTY LINE	STORM DRAIN MANHOLE	SIGN
ADJACENT PROPERTY LINE	SANITARY SEWER MANHOLE	TREE W/ SIZE AND ELEVATION
CENTERLINE	CLEAN OUT	SPOT ELEVATION
MONUMENT LINE	GAS METER	CONTOUR
BUILDING LINE W/ DOOR	UTILITY POLE W/ GUY WIRE	INDEX CONTOUR
BUILDING OVERHANG	VALVE	CURB
FOUND MONUMENT AS NOTED	CATCH BASIN / DROP INLET	CURB & GUTTER
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TRANSFORMER	UTILITY BOX (SIZE VARIES)	EDGE OF PAVEMENT
FIRE HYDRANT	MONITORING WELL	SANITARY SEWER

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No.	Description	Date
1	USE PERMIT APPLICATION	2021.03.25
2	USE PERMIT RESUBMITTAL	2021.09.13
3	USE PERMIT RESUBMITTAL	2021.12.17
4	DESIGN REVIEW COMMITTEE MEETING 1	2022.02.07

Key Plan:

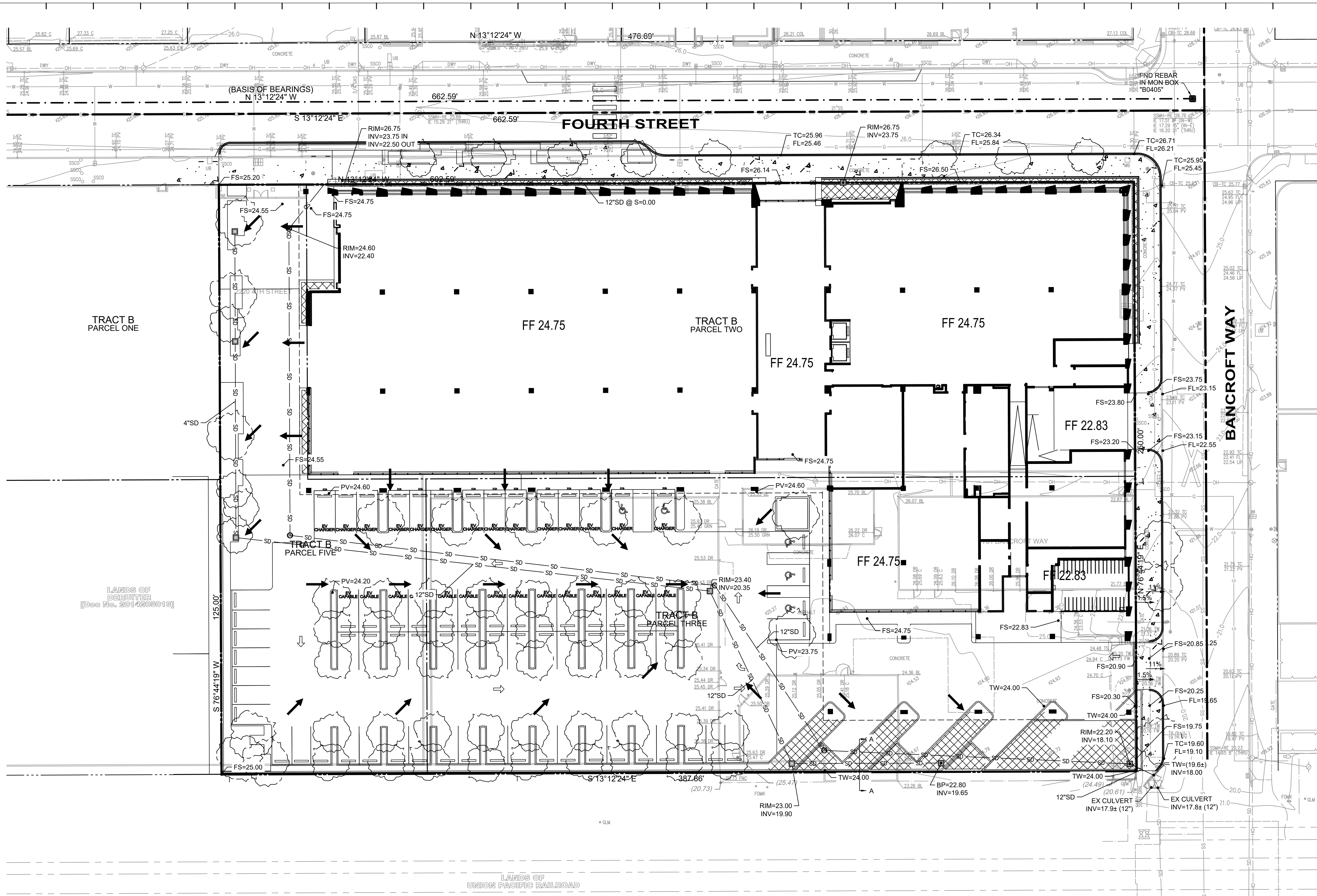
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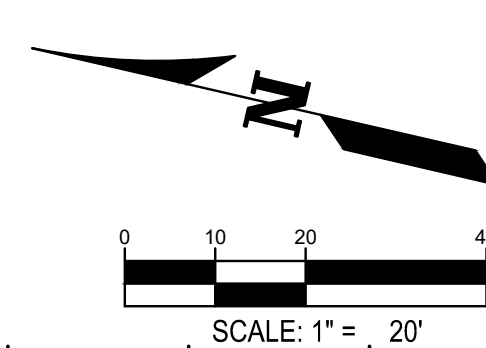
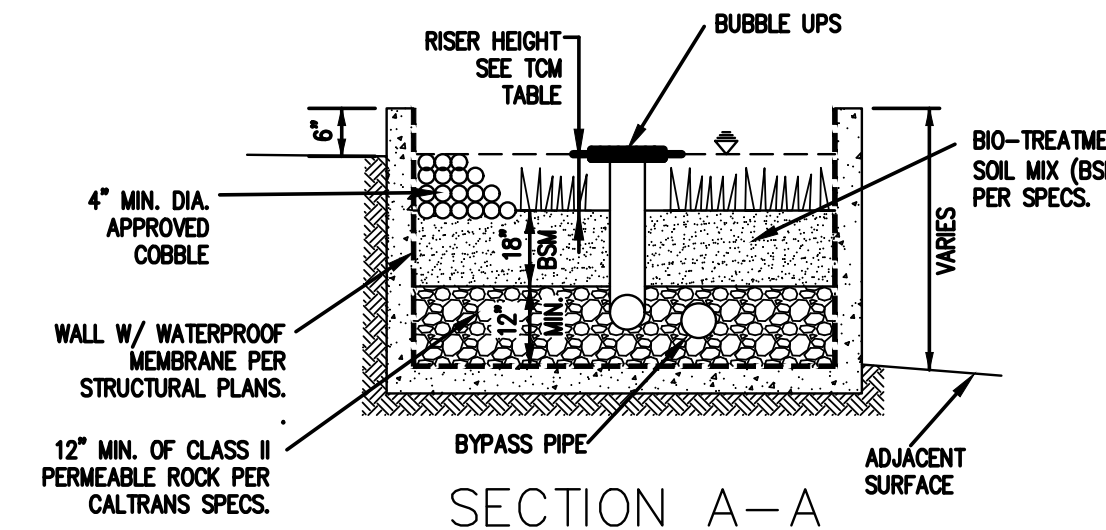
CONCEPTUAL GRADING PLAN

Project No.: 220175
 Sheet No.:
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 Checked By: MM
 Scale:
CP3.00



LEGEND:

	PROPERTY LINE		PROPOSED SWALE		FLOW DIRECTION
	PROPOSED 5' CONTOUR		PROPOSED CURB		HIGH POINT
	PROPOSED 1' CONTOUR		EXISTING CURB		LOW POINT
	EXISTING 5' CONTOUR		EXISTING OVERHEAD ELECTRIC LINE		MATCH EXISTING
	EXISTING 1' CONTOUR		EXISTING NATURAL GAS LINE		GRADE BREAK
	PROPOSED STORM LINE		EXISTING SANITARY LINE		TOP OF WALL
	EXISTING STORM LINE		EXISTING WATER LINE		FINISH GRADE AT WALL
	PROPOSED STORM INLET		PROPOSED SPOT ELEVATION (AT FLOWLINE UNLESS OTHERWISE INDICATED)		REMOVE AND REPLACE CURB, GUTTER AND SIDEWALK
	EXISTING STORM INLET		PROPOSED SLOPE AND DIRECTION		PROPOSED STORMWATER TREATMENT AREA



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ROCKRIDGE GEOTECHNICAL
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CONCEPTUAL STORMWATER MANAGEMENT PLAN

Project No.:

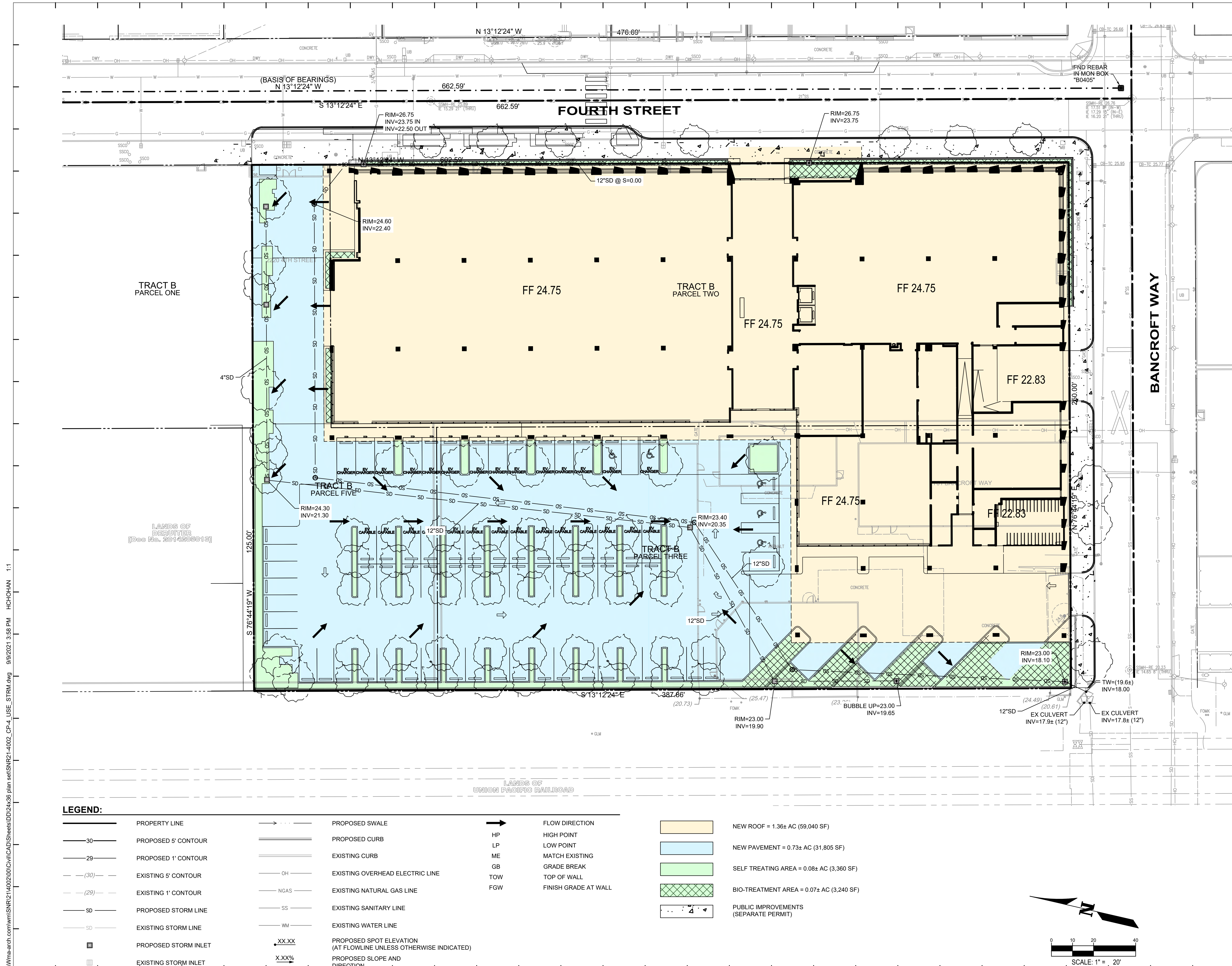
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Checked By: MM

Scale:

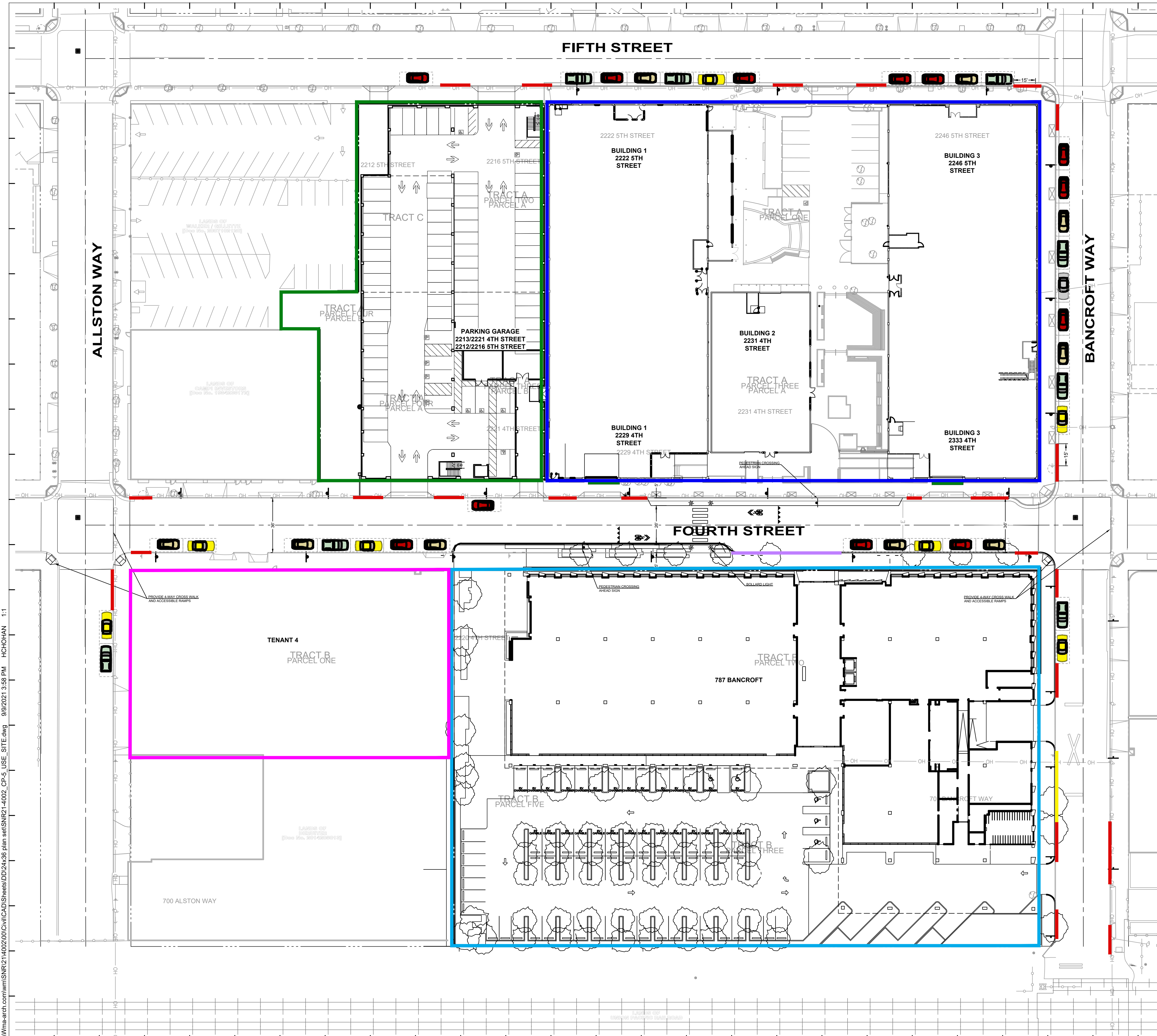
CP4.00



LEGEND:

	PROPERTY LINE		PROPOSED SWALE		FLOW DIRECTION		NEW ROOF = 1.36± AC (59,040 SF)
	PROPOSED 5' CONTOUR		PROPOSED CURB		HIGH POINT		NEW PAVEMENT = 0.73± AC (31,805 SF)
	PROPOSED 1' CONTOUR		EXISTING CURB		LOW POINT		SELF TREATING AREA = 0.08± AC (3,360 SF)
	EXISTING 5' CONTOUR		EXISTING OVERHEAD ELECTRIC LINE		MATCH EXISTING		BIO-TREATMENT AREA = 0.07± AC (3,240 SF)
	EXISTING 1' CONTOUR		EXISTING NATURAL GAS LINE		GRADE BREAK		PUBLIC IMPROVEMENTS (SEPARATE PERMIT)
	PROPOSED STORM LINE		EXISTING SANITARY LINE		TOP OF WALL		
	EXISTING STORM LINE		EXISTING WATER LINE		FINISH GRADE AT WALL		
	PROPOSED STORM INLET		PROPOSED SPOT ELEVATION (AT FLOWLINE UNLESS OTHERWISE INDICATED)				
	EXISTING STORM INLET		PROPOSED SLOPE AND DIRECTION				

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LEGEND

- NO PARKING - RED CURB
- LOADING ZONE - YELLOW CURB
- PASSENGER LOADING ZONE - WHITE CURB
- PARKING STALL
- PHASE 1
- PHASE 2
- PHASE 3
- PHASE 4
- EXISTING FH WITH 3' RADIUS CLEARANCE
- PROPOSED 'NO OVERNIGHT PARKING' SIGN - SEE DETAIL ON THIS SHEET



NO OVERNIGHT PARKING SIGN

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 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

BUILDER
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 DUBLIN, CA 94568

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 MEYERS+ ENGINEERS
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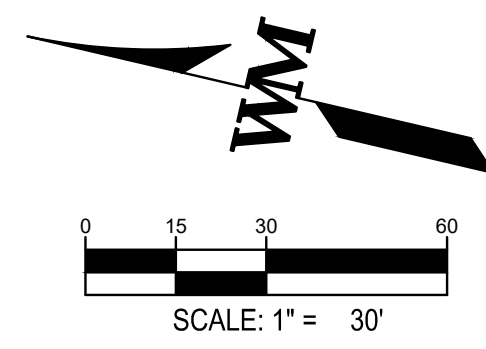
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Sheet Name:
OVERALL SITE PLAN

Project No.: 220175	Sheet No.:
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 DUBLIN, CA 94568

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 WARE MALCOMB
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MEP ENGINEER
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 ROCKRIDGE GEOTECHNICAL
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LAB MASTERPLAN

Project No.: 220175	Sheet No.:
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PROPOSED ROOF LEVEL SITE PLAN 01
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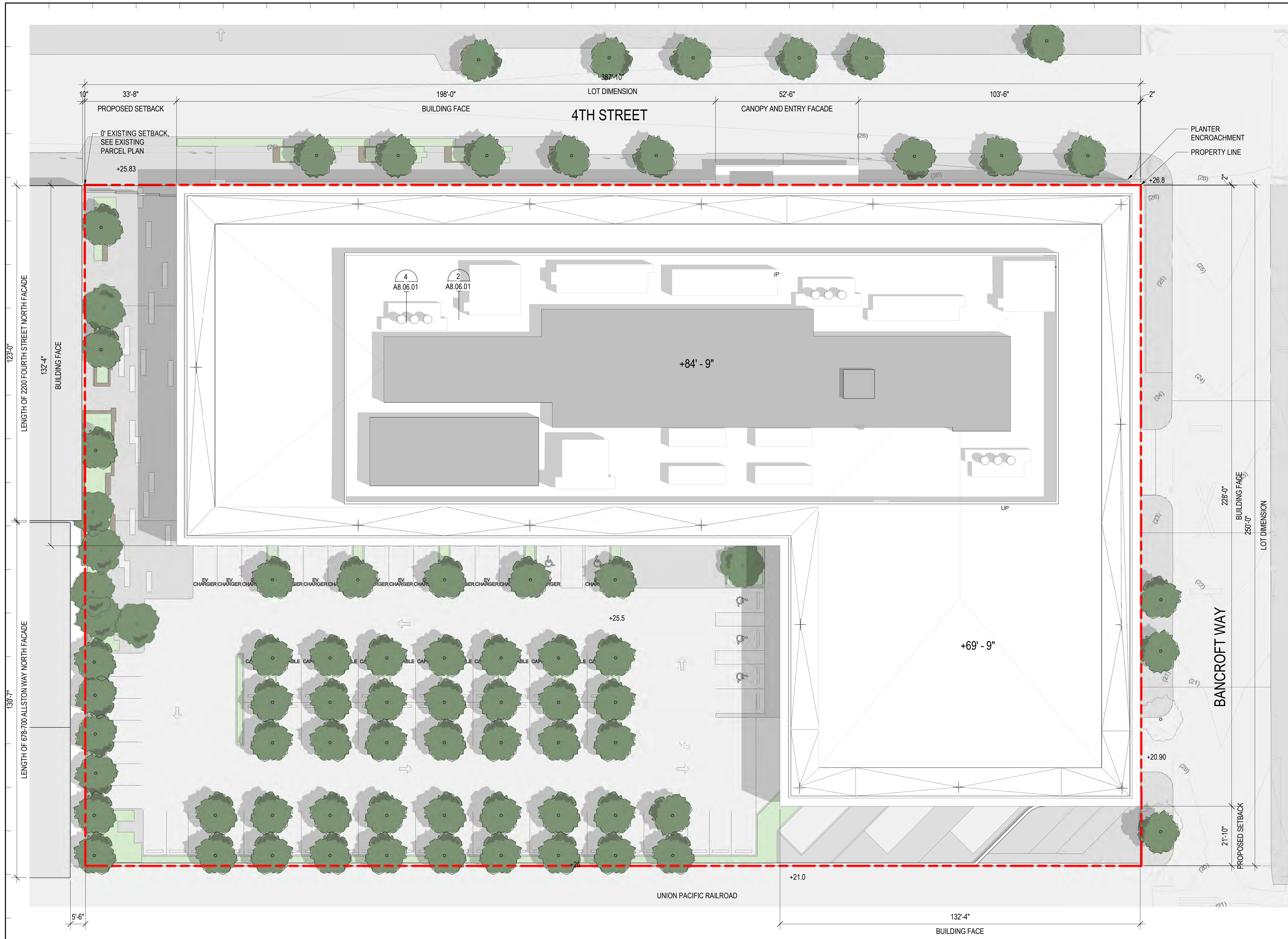
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Sheet Name:
OVERALL SITE PLAN

Project No.:	Sheet No.:
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SITE PLAN 01
 SCALE: 1/16" = 1'-0"

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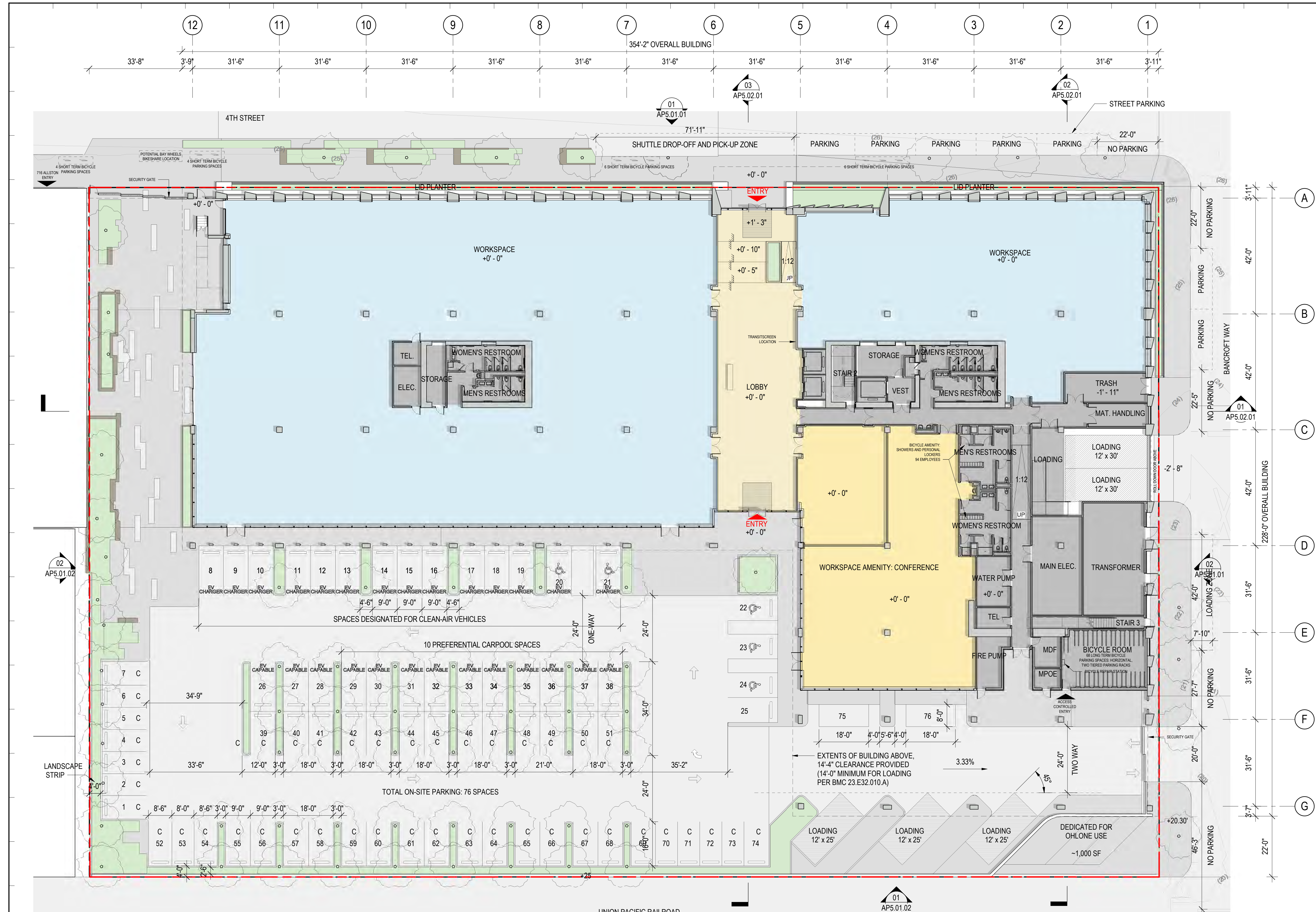
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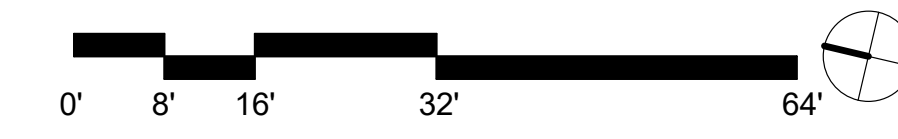
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OVERALL FLOOR PLAN - LEVEL 01

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- GENERAL NOTES:
- ALL SITE ELEVATIONS ARE BASED ON CIVIL ENGINEER'S SURVEY
 - SEE NOTES ON C.01 FOR SURVEY POINT'S RELATIONSHIP TO BERKELEY CITY DATUM
 - "NO PARKING" DESIGNATED BY RED CURB
 - TRUCK ACCESS TO PARKING LOT RESTRICTED TO DL-23 OR SMALLER SIZE TRUCKS
 - SPACES LABELED AS "EV CHARGER" WILL HAVE LEVEL 2 ELECTRIC CHARGING STATIONS INSTALLED ON DAY ONE PER BERKELEY REACH CODE
 - SPACES LABELED AS "EV CAPABLE" WILL BE EQUIPPED WITH EV CONNECTING RACEWAYS



LEVEL 01 FLOORPLAN 01

SCALE: 1/16" = 1'-0" 01

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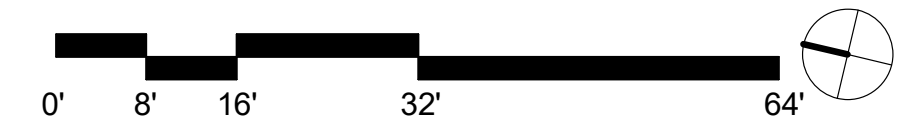
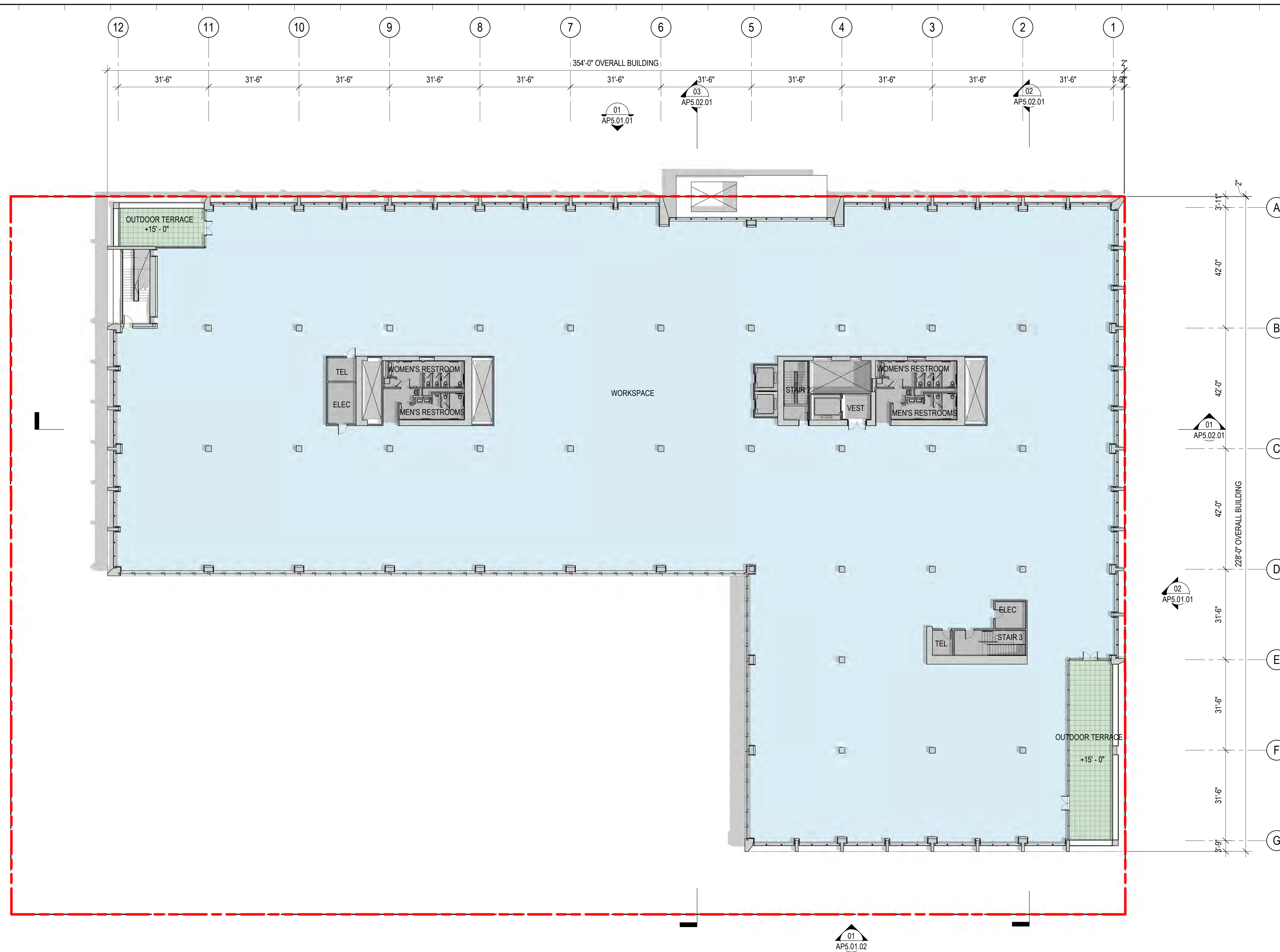
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OVERALL FLOOR PLAN - LEVEL 02

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LEVEL 02 FLOORPLAN 01
 SCALE: 1/16" = 1'-0"

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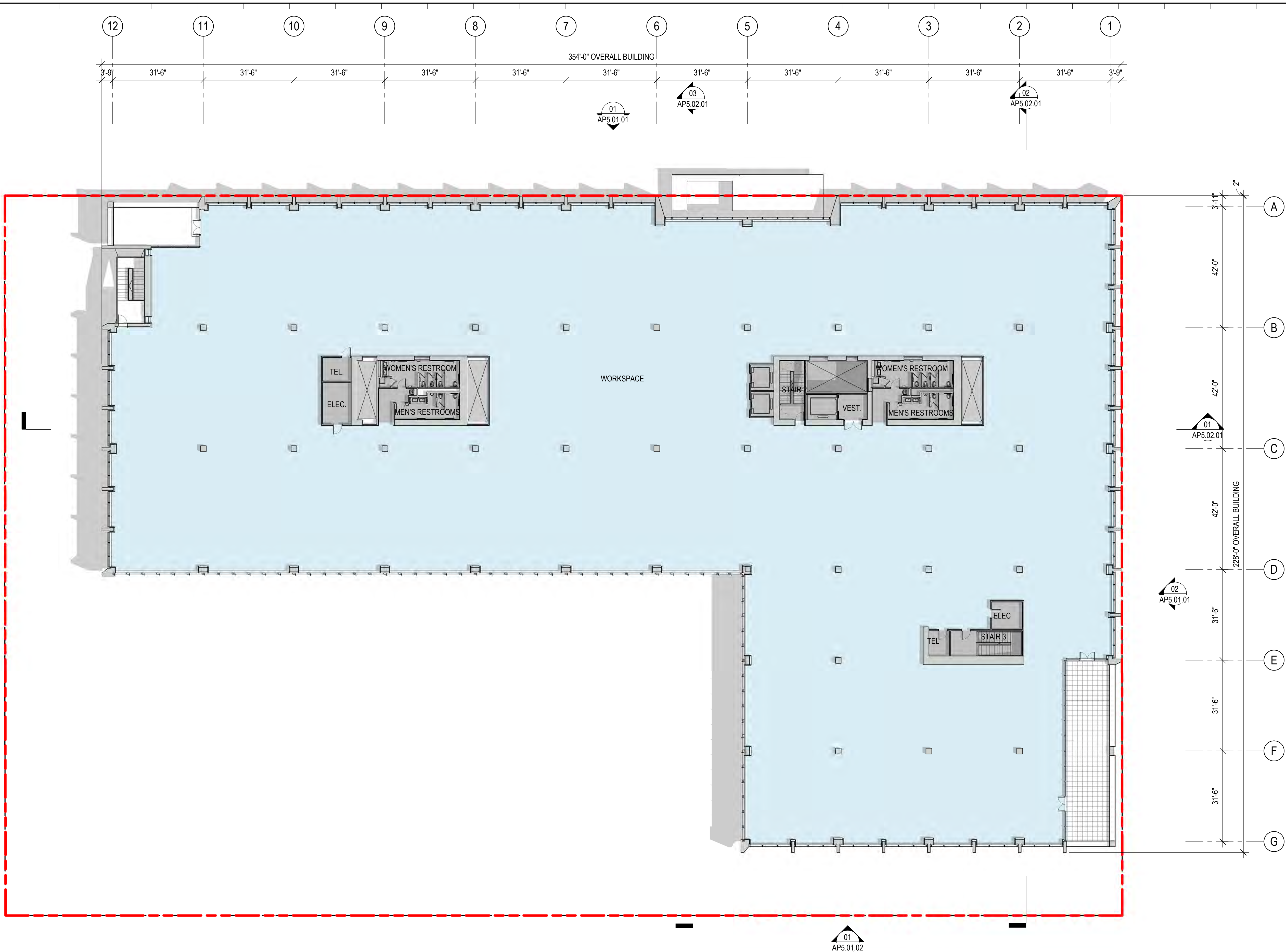
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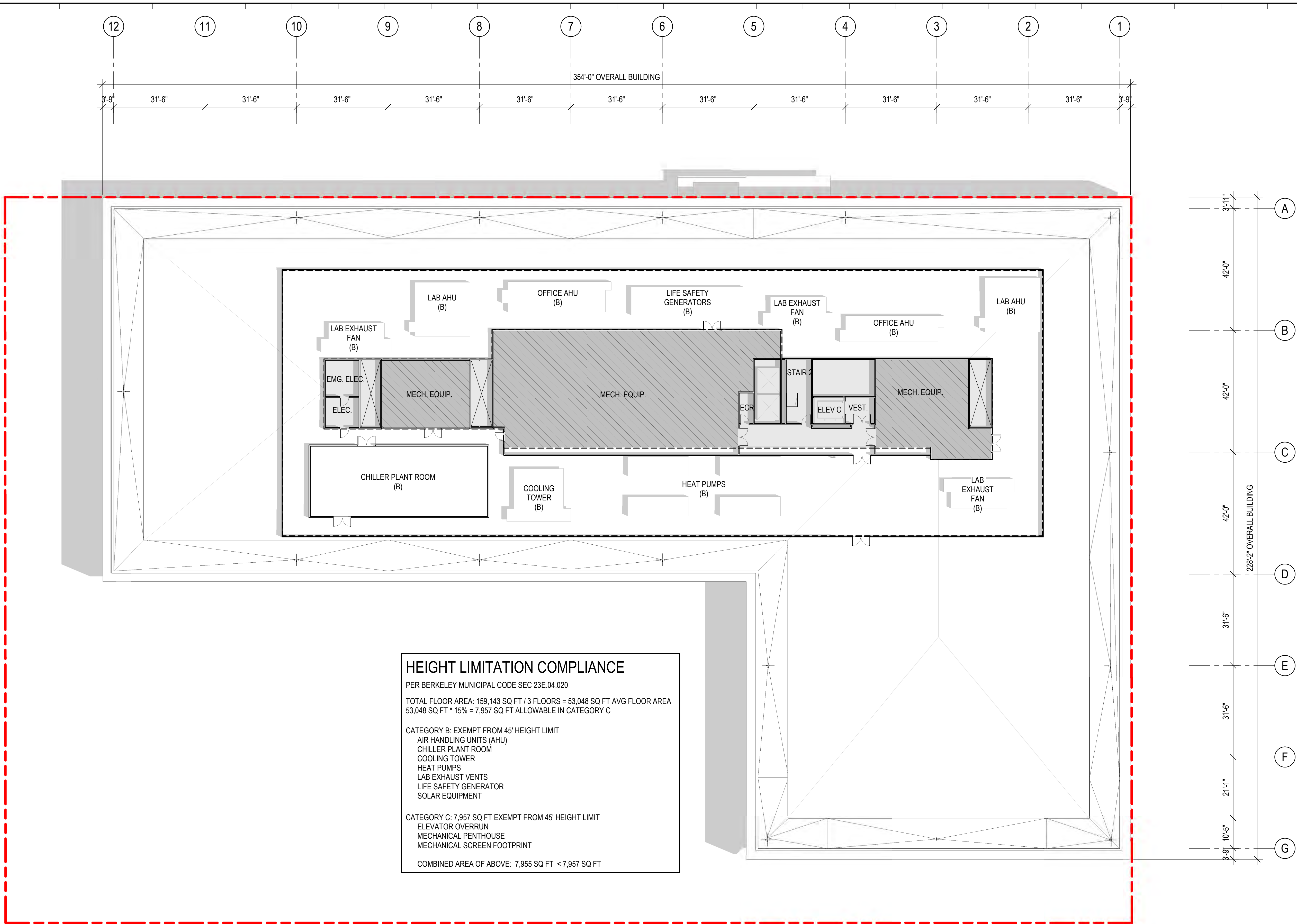
OVERALL FLOOR PLAN - LEVEL 03

Project No.: 220175	Sheet No.: AP2.03.00
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LEVEL 03 FLOORPLAN 01
 SCALE: 1/16" = 1'-0"

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HEIGHT LIMITATION COMPLIANCE
 PER BERKELEY MUNICIPAL CODE SEC 23E.04.020
 TOTAL FLOOR AREA: 159,143 SQ FT / 3 FLOORS = 53,048 SQ FT AVG FLOOR AREA
 53,048 SQ FT * 15% = 7,957 SQ FT ALLOWABLE IN CATEGORY C

CATEGORY B: EXEMPT FROM 45' HEIGHT LIMIT
 AIR HANDLING UNITS (AHU)
 CHILLER PLANT ROOM
 COOLING TOWER
 HEAT PUMPS
 LAB EXHAUST VENTS
 LIFE SAFETY GENERATOR
 SOLAR EQUIPMENT

CATEGORY C: 7,957 SQ FT EXEMPT FROM 45' HEIGHT LIMIT
 ELEVATOR OVERRUN
 MECHANICAL PENTHOUSE
 MECHANICAL SCREEN FOOTPRINT

COMBINED AREA OF ABOVE: 7,955 SQ FT < 7,957 SQ FT

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 MEYERS+ ENGINEERS
 99 BATTERY STREET, SUITE 500
 SAN FRANCISCO, CA 94111

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 ROCKRIDGE GEOTECHNICAL
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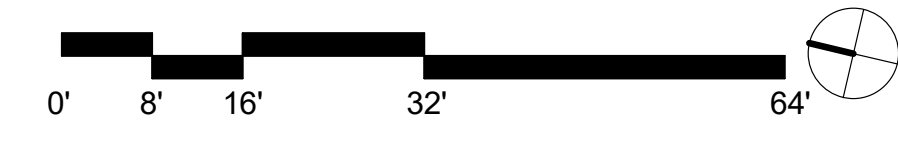
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Sheet Name:
OVERALL FLOOR PLAN - MECHANICAL PENTHOUSE

Project No.: 220175
 Sheet No.: AP2.04.00
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LEVEL 04 MECHANICAL PENTHOUSE PLAN 01
 SCALE: 1/16" = 1'-0"

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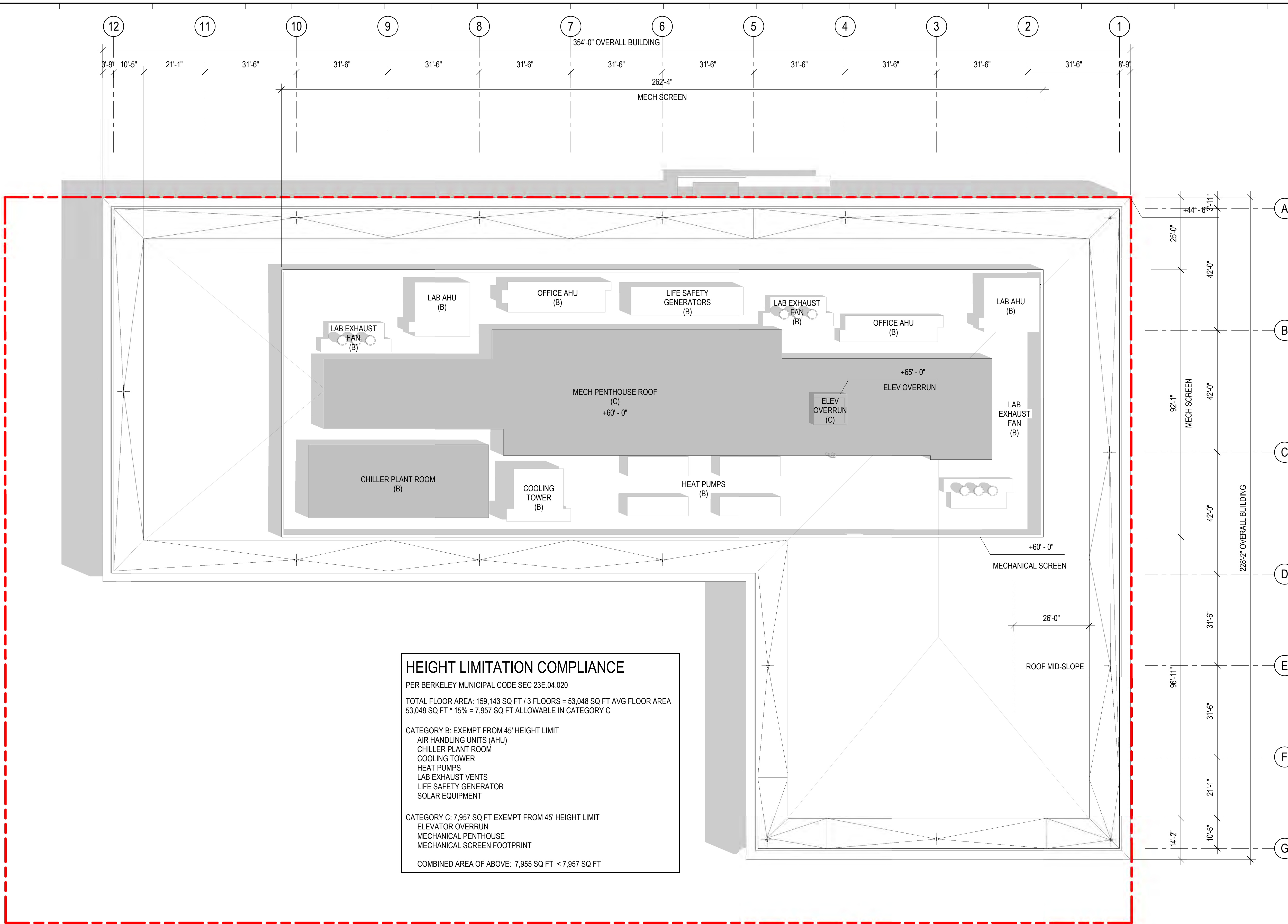
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OVERALL FLOOR PLAN - ROOF

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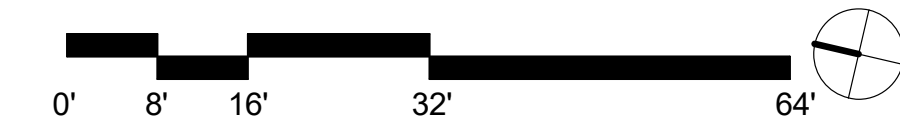


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CATEGORY C: 7,957 SQ FT EXEMPT FROM 45' HEIGHT LIMIT
 ELEVATOR OVERRUN
 MECHANICAL PENTHOUSE
 MECHANICAL SCREEN FOOTPRINT

COMBINED AREA OF ABOVE: 7,955 SQ FT < 7,957 SQ FT



ROOF PLAN 01
 SCALE: 1/16" = 1'-0"

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 ONE MARITIME PLAZA
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 DUBLIN, CA 94568

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 WARE MALCOMB
 4883 CHABOT DRIVE #300
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 ROCKRIDGE GEOTECHNICAL
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Sheet Name:
EXISTING ELEVATIONS

Project No.: 220175	Sheet No.: AP5.00.01
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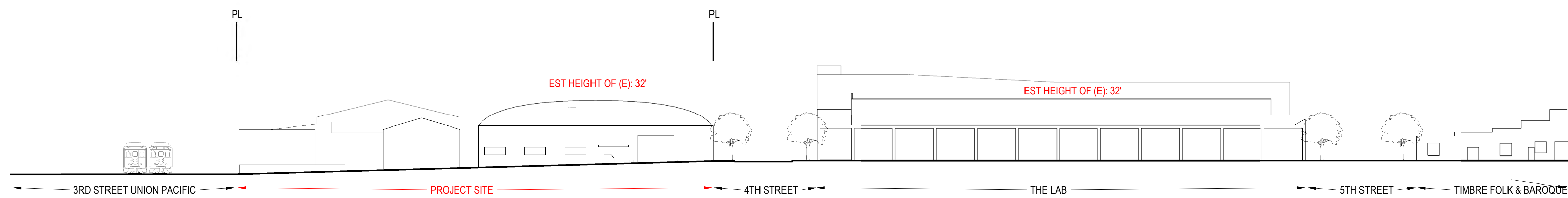
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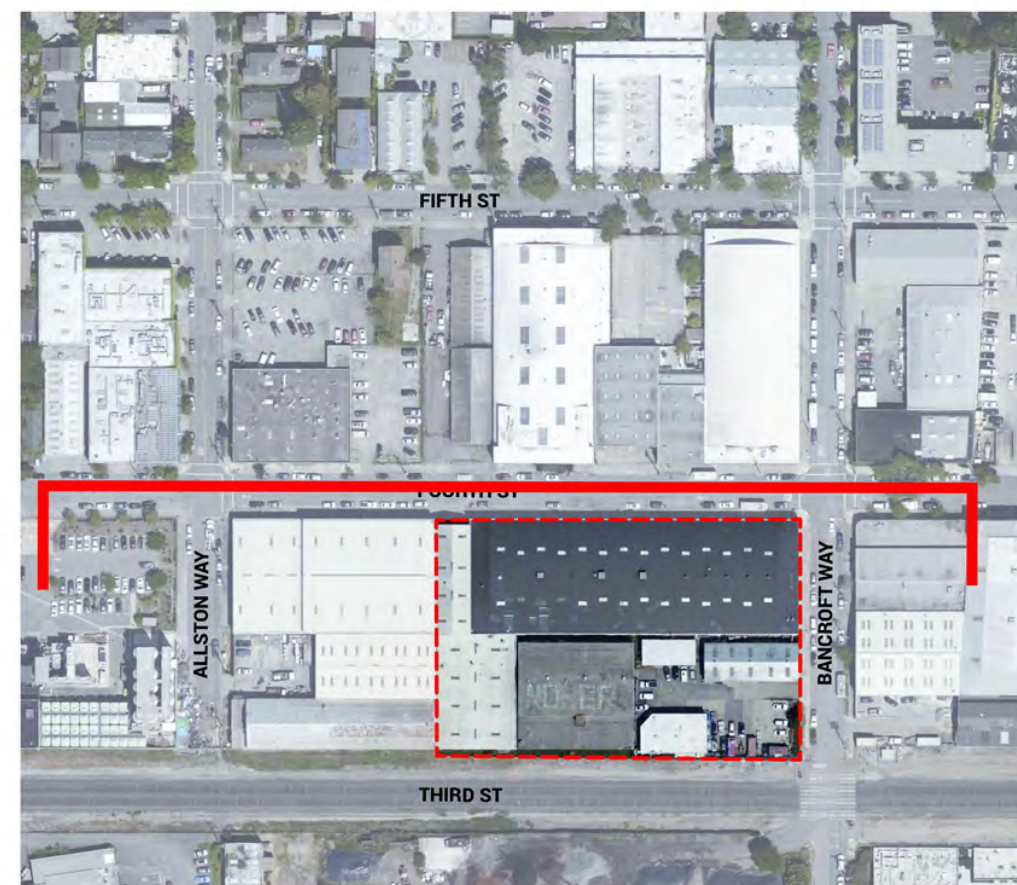
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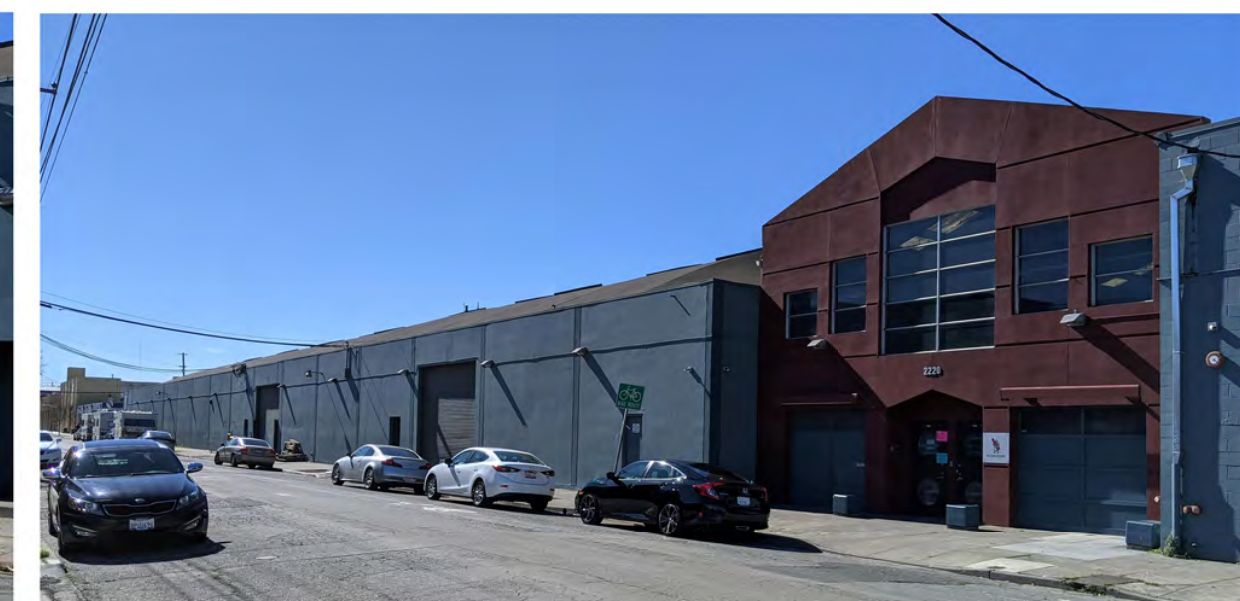
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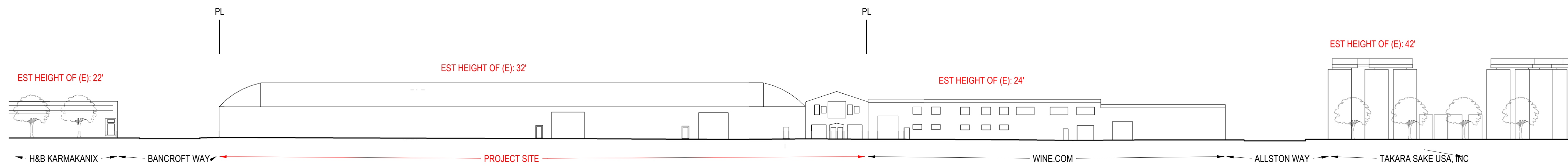
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EAST SITE ELEVATION EXISTING 01
 SCALE: 1/32" = 1'-0"



01 3RD STREET UNION PACIFIC



02 TIMBRE FOLK & BAROQUE

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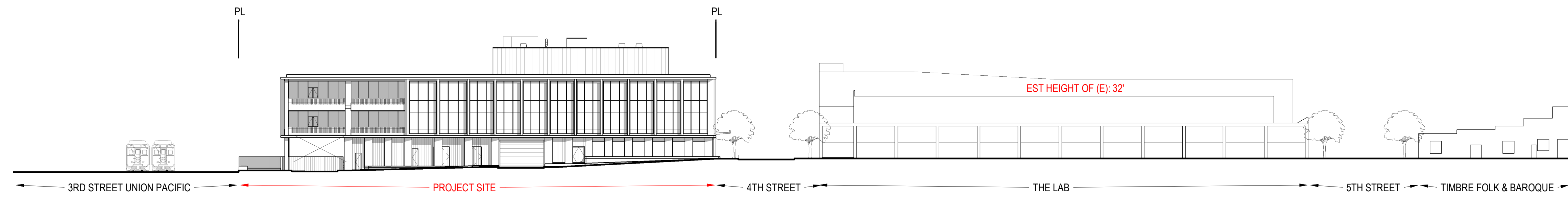
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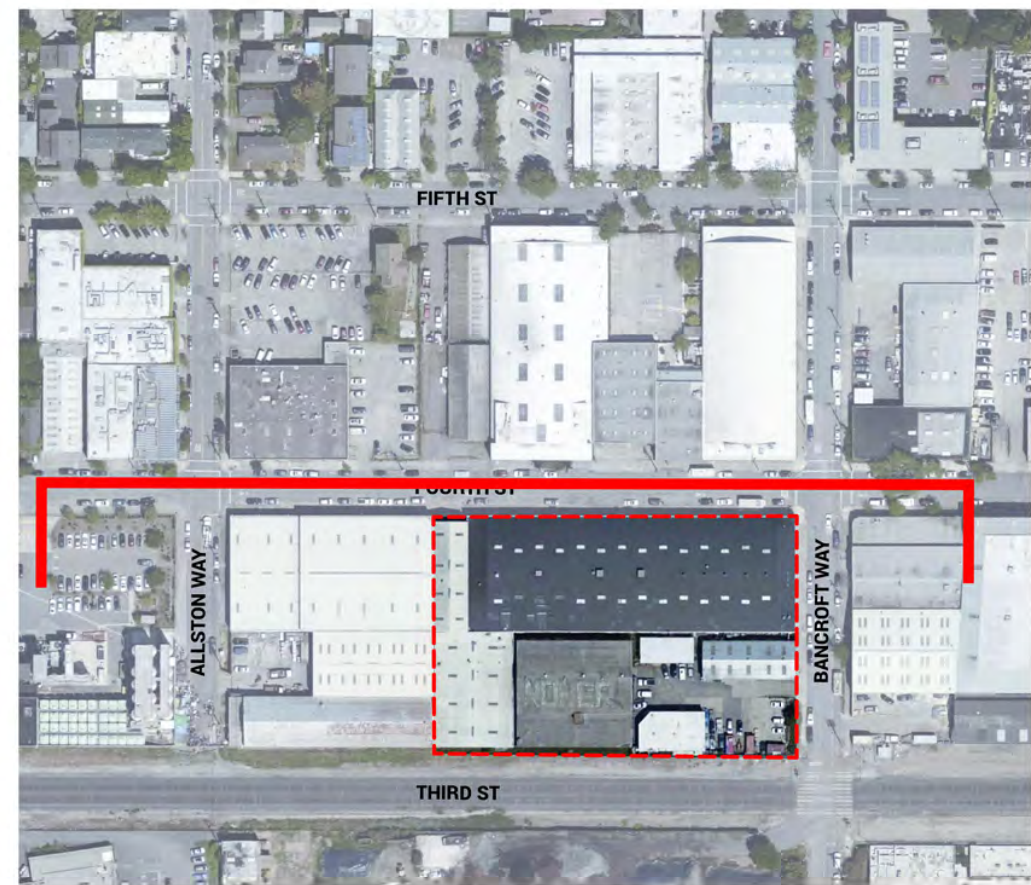
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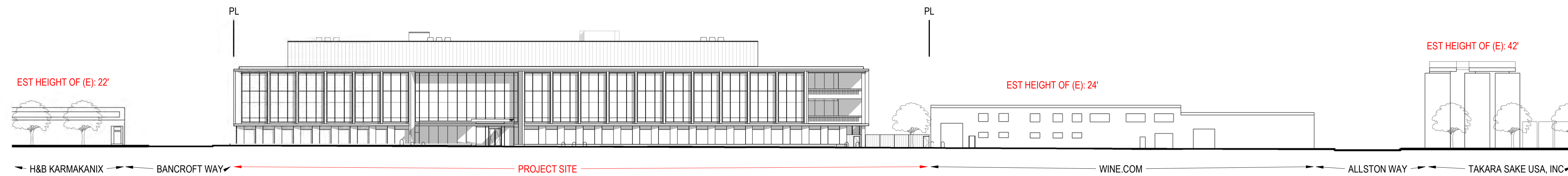
SOUTH SITE ELEVATION 02
 SCALE: 1/32" = 1'-0"



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EAST SITE ELEVATION 01
 SCALE: 1/32" = 1'-0"

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SITE ELEVATIONS

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	50% SCHEMATIC DESIGN	2021.04.30
	90% SCHEMATIC DESIGN	2021.05.21
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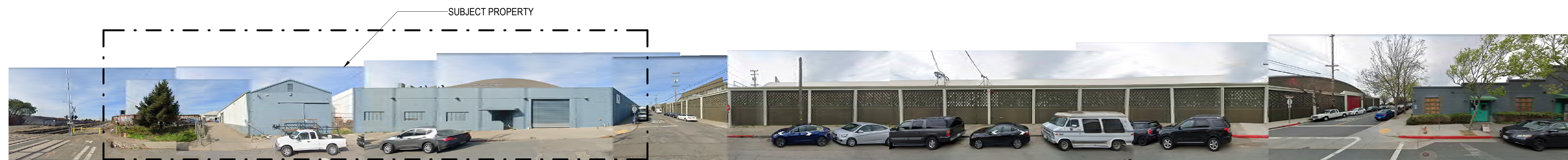
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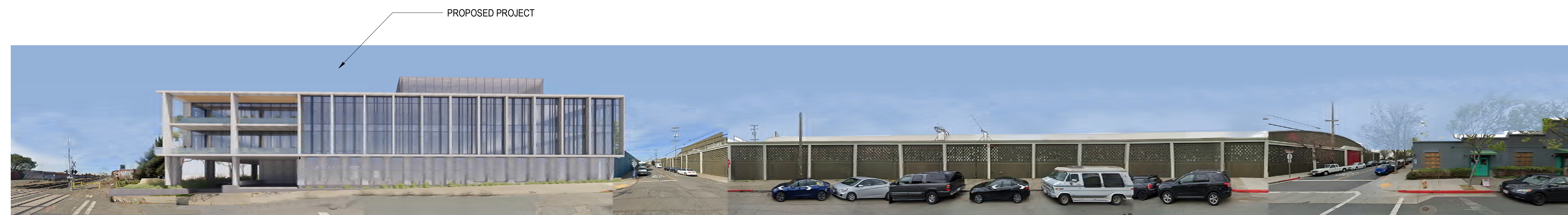
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Sheet Name:
STREET STRIP ELEVATIONS

Project No.: 220175
 Sheet No.:
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 Scale:
AP5.00.03



STREET STRIP, BANCROFT WAY, LOOKING NORTH - EXISTING



STREET STRIP, BANCROFT WAY, LOOKING NORTH - PROPOSED



STREET STRIP, 4TH STREET, LOOKING WEST - EXISTING



STREET STRIP, 4TH STREET, LOOKING WEST - PROPOSED

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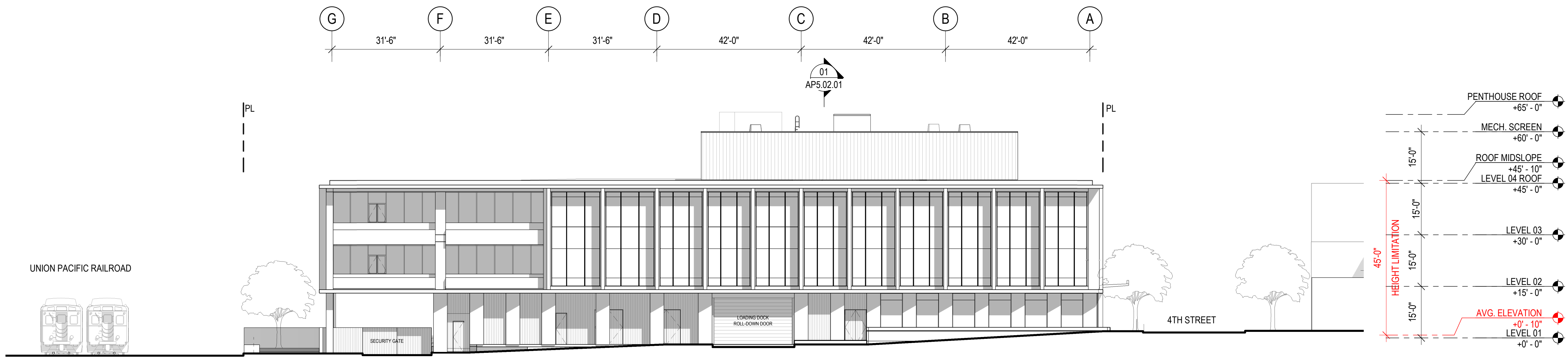
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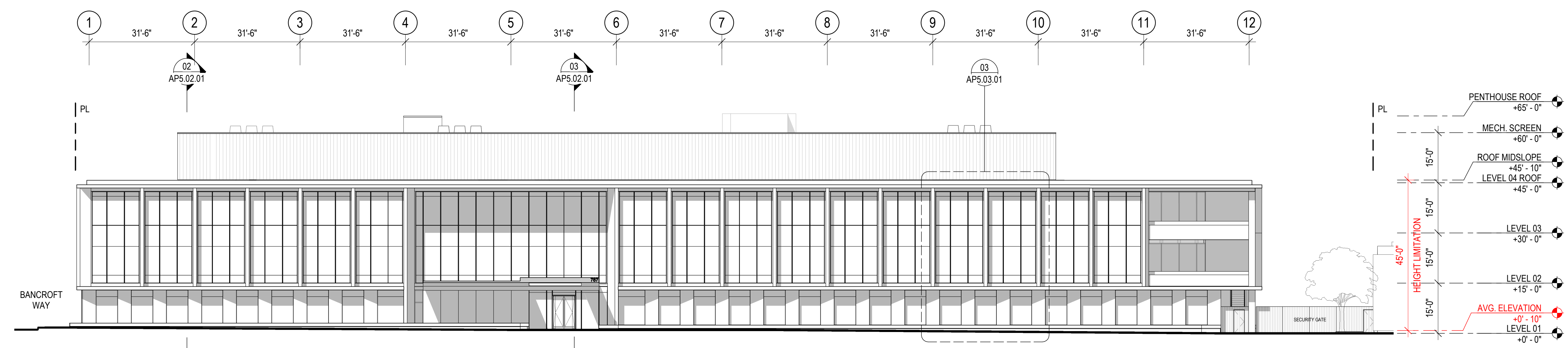
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EAST AND SOUTH ELEVATIONS

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220175	AP5.01.01
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SOUTH ELEVATION 02
 SCALE: 1/16" = 1'-0"



EAST ELEVATION 01
 SCALE: 1/16" = 1'-0"

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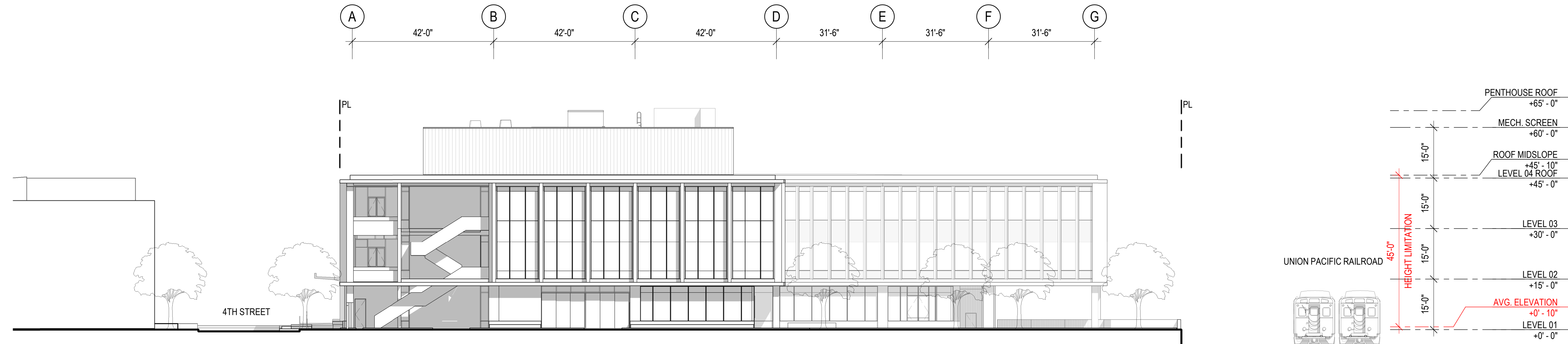
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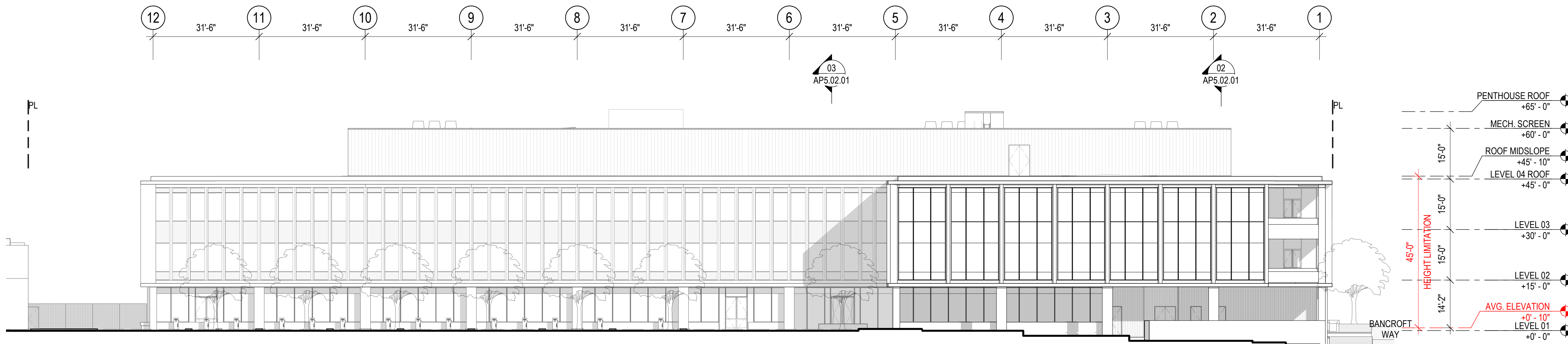
Sheet Name:

WEST AND NORTH ELEVATIONS

Project No.:	Sheet No.:
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NORTH ELEVATION 02
 SCALE: 1/16" = 1'-0"



WEST ELEVATION 01
 SCALE: 1/16" = 1'-0"

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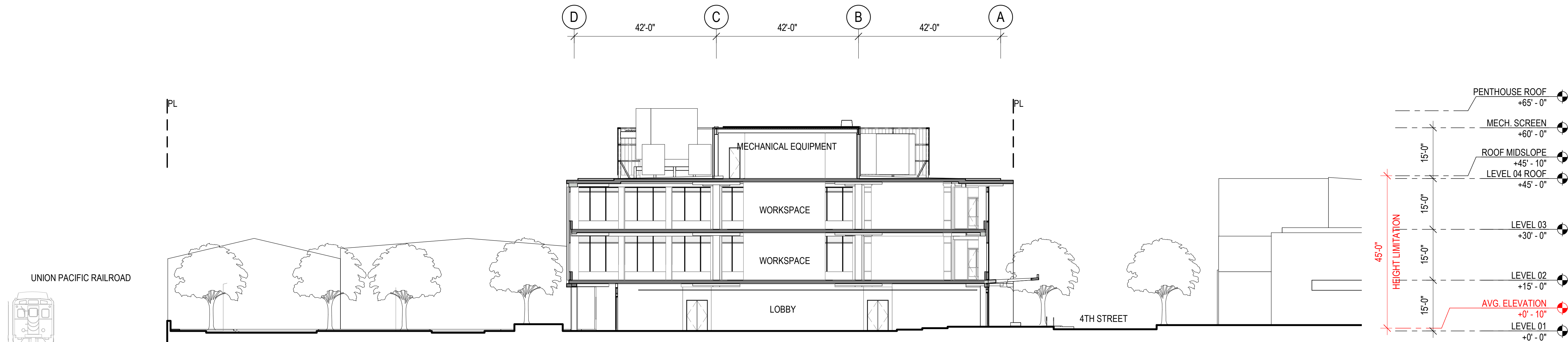
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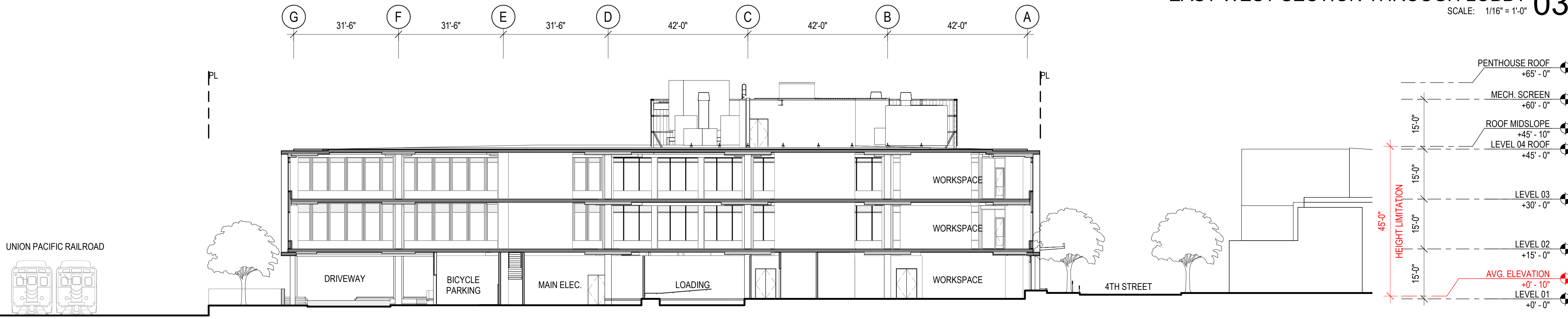
SECTIONS

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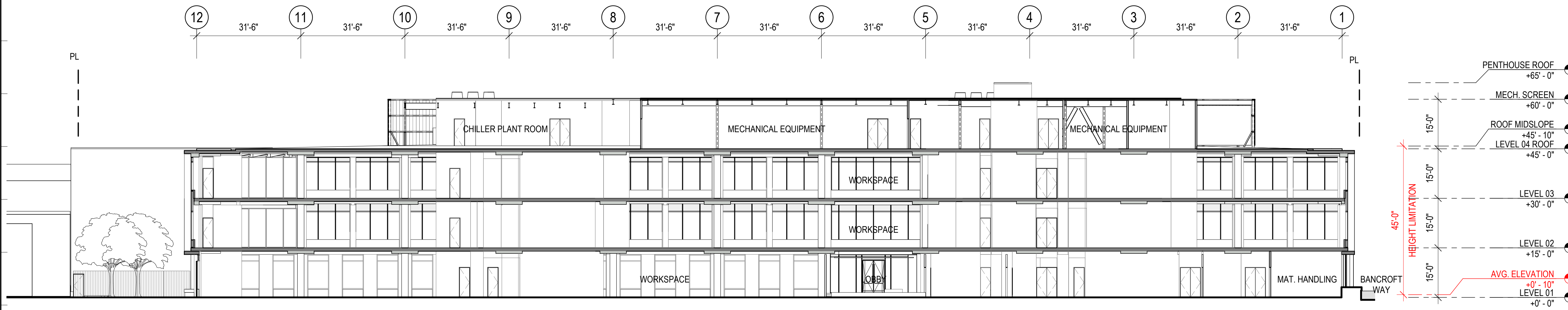
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EAST WEST SECTION THROUGH LOBBY 03
 SCALE: 1/16" = 1'-0"



EAST WEST SECTION THROUGH DRIVEWAY 02
 SCALE: 1/16" = 1'-0"



NORTH SOUTH SECTION 1 01
 SCALE: 1/16" = 1'-0"

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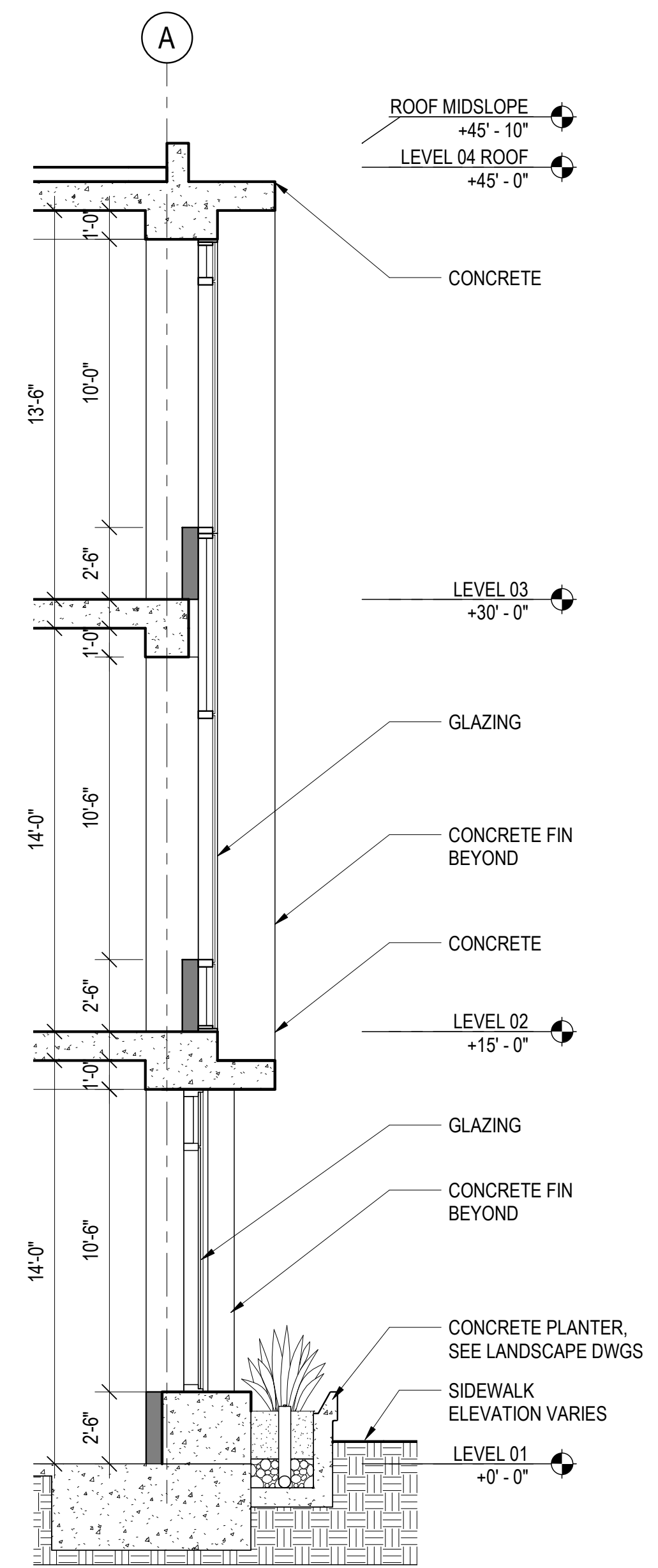
EXTERIOR VIEW AT TYPICAL FACADE (4TH STREET)



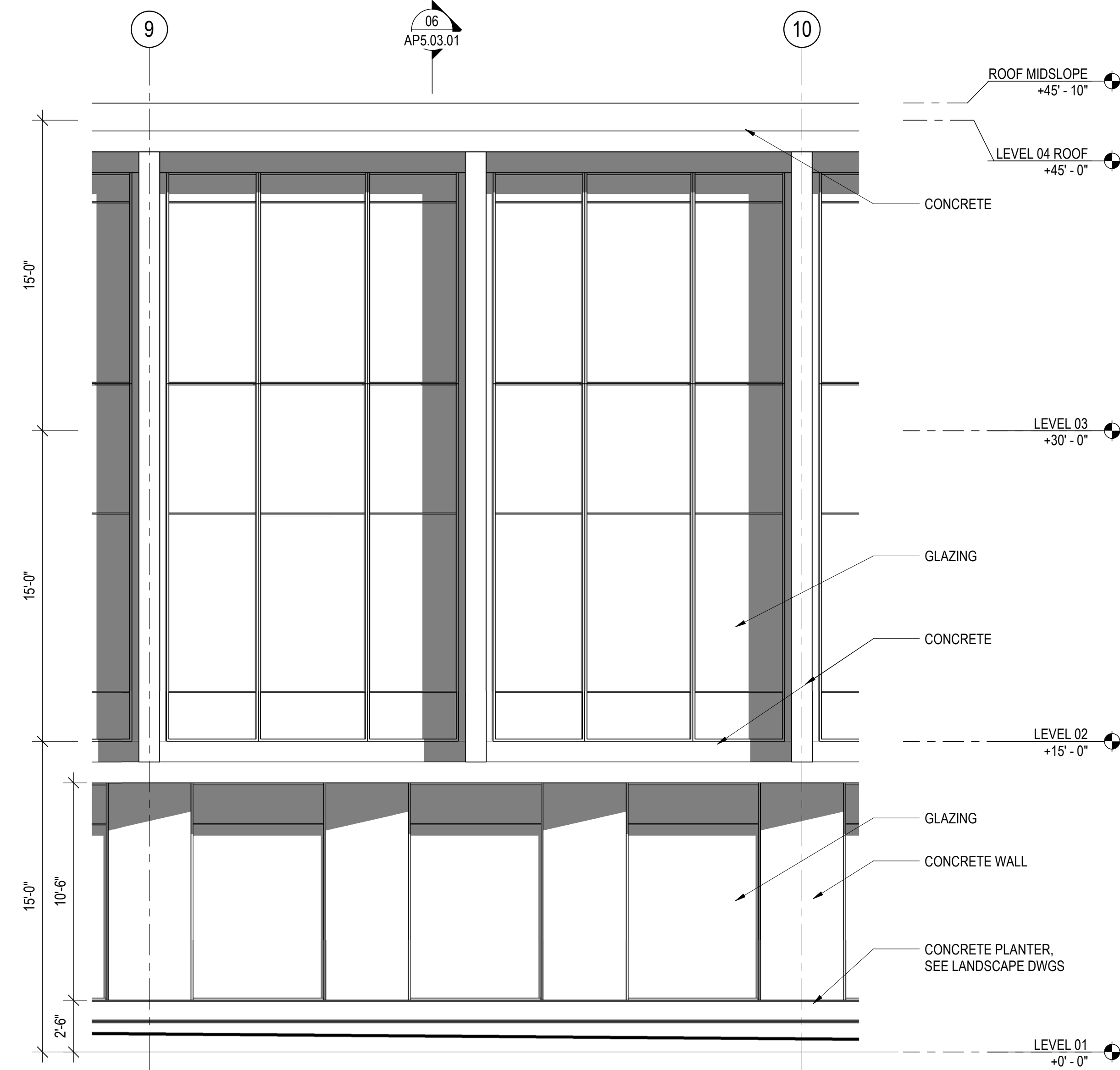
EXTERIOR VIEW AT EAST ENTRY



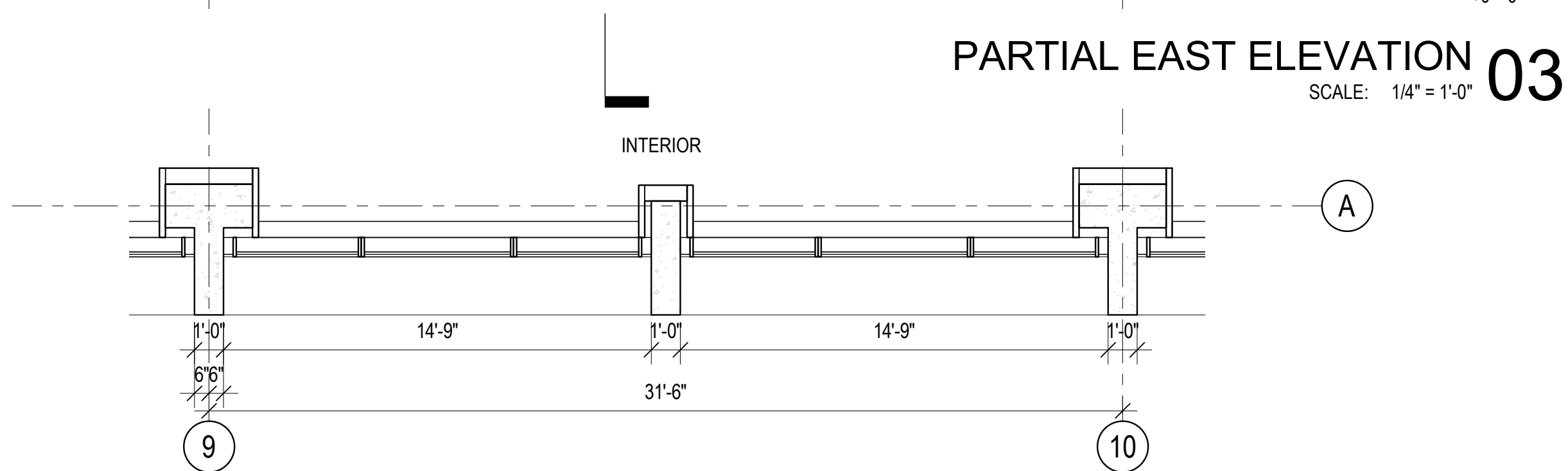
EXTERIOR VIEW AT SE CORNER



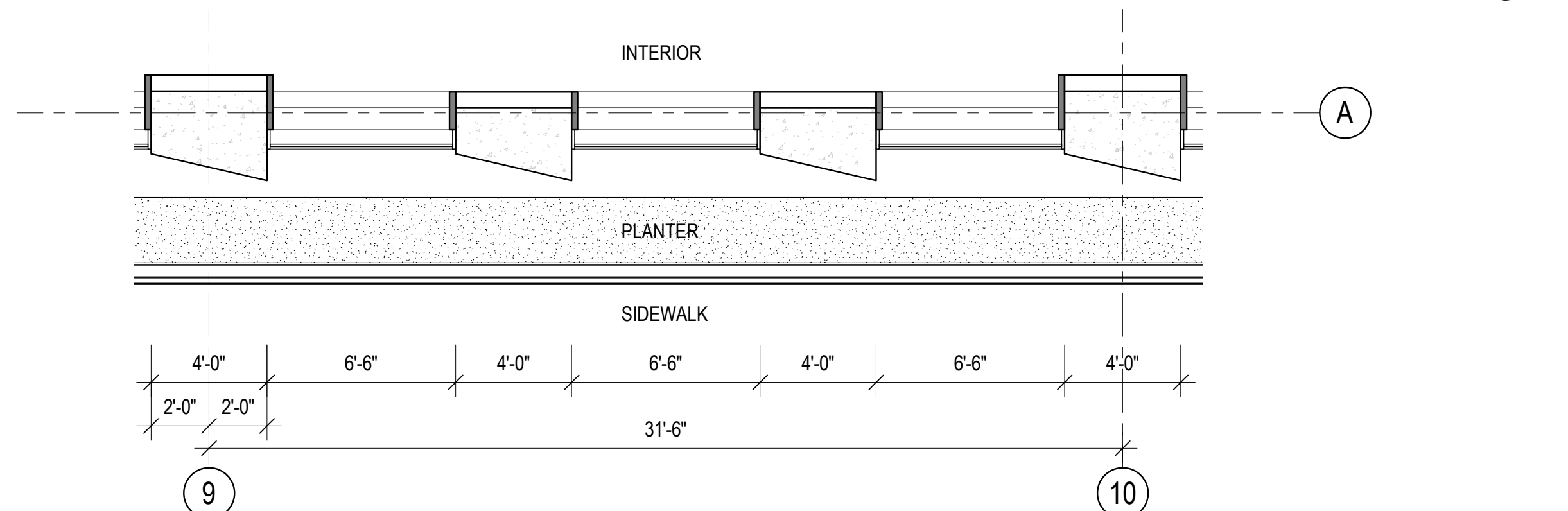
PARTIAL SECTION AT EAST 06
 SCALE: 1/4" = 1'-0"



PARTIAL EAST ELEVATION 03
 SCALE: 1/4" = 1'-0"



LEVEL 02 PARTIAL PLAN AT EAST TYPICAL 02
 SCALE: 1/4" = 1'-0"



LEVEL 01 PARTIAL PLAN AT EAST TYPICAL 01
 SCALE: 1/4" = 1'-0"

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EXTERIOR WALL DETAILS

Project No.: 220175
 Sheet No.: AP5.03.01
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**PLANTING LEGEND
 TREE LEGEND**

PLANTING LEGEND							
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	NATIVE	SPACING	NOTES
	ACER RUBRUM 'OCTOBER GLORY'	RED MAPLE	36" BOX	M	N	AS SHOWN	STREET TREE
	ACER JAPONICUM	JAPANESE MAPLE	36" BOX	M	N	AS SHOWN	PLAZA TREE SPECIMEN TREE
	PLATANUS HYBRIDA	LONDON PLANE TREE	48" BOX	M	N	AS SHOWN	PLAZA TREE
	ULMUS HYBRID 'FRONTIER'	FRONTIER ELM	36" BOX	M	N	AS SHOWN	PARKING LOT TREE

**PLANTING LEGEND
 SHRUB/GROUND COVER LEGEND**

PLANTING LEGEND					
SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	WUCOLS	NOTES
	CAREX PANSA	CALIFORNIA BEACH DUNE	FLATS	L	LAWN
	ARCTOSTAPHYLOS 'EMERALD CARPET'	CARPET MANZANITA	FLATS	L	GROUND COVER
	LOMANDRA LONGIFOLIA 'BREEZE'	DWARF MAT RUSH	1 GAL	L	ORNAMENTAL GRASS
	MUHLENBERGIA RIGENS	DEER GRASS	5 GAL	L	ORNAMENTAL GRASS
	PENNISETUM ALOPECUROIDES	FOUNTAIN GRASS	5 GAL	L	ORNAMENTAL GRASS
	CAREX TUMULICOLA	BERKELEY SEDGE	1 GAL	L	FLOW THROUGH PLANTER
	IRIS PACIFIC COAST HYBRID	PACIFIC COAST HYBRID IRIS	1 GAL	L	FLOW THROUGH PLANTER
	LIGUSTRUM OVALIFOLIUM	CALIFORNIA PRIVET	5 GAL	L	HEDGE - MAINTAIN 3' - 6" OF HEDGE HEIGHT

SHADED PARKING LEGEND

SHADED PARKING LEGEND		
LEGEND	NAME	NOTES
	OPEN AIR PARKING PAVED AREA	ONLY OPEN AIR SURFACE PARKING HAS BEEN INCLUDED IN THIS TREE CANOPY COVERAGE CALCULATION
	TRISTANIA LAURINA 'ELEGANT' SEE PLANTING PLAN	DIAMETER AT FULL MATURITY: 20'
	PARKING AREA SHADED BY TREE CANOPY	DIAMETER AT FULL MATURITY: 20'
	PARKING AREA SHADED BY BUILDING/ STRUCTURE	THIS PART IS NOT INCLUDED IN THE CALCULATION

HARDSCAPE LEGEND

HARDSCAPE LEGEND					
LEGEND	MATERIALS	MODEL / MANUFACTURER	COLOR / FINISH		COMMENTS
P1	CONCRETE PAVING / INTEGRAL COLOR	-	NATURAL GRAY	MEDIUM / SANDBLAST	SEE DETAILS
P2A	CONCRETE UNIT PAVER A	HANOVER	NATURAL	TUDOR FINISH	SEE DETAILS
P2B	CONCRETE UNIT PAVER B	HANOVER	LIMESTONE GRAY	TUDOR FINISH	SEE DETAILS
P4	CONCRETE CURB	-	-	-	PER CIVIL
P5	SIDEWALK CONCRETE PAVING	-	-	-	PER CIVIL
P6	CRUSHED GRANITE	LYNGSO GARDEN MATERIALS	MIXED COLOR	-	SEE DETAILS
P7	PERMEABLE INTERLOCKING CONCRETE PAVER	HANOVER	LIME STONE GRAY	TUDOR FINISH	SEE DETAILS
P8	ASPHALT	-	-	-	PER CIVIL
PA	PLANTING AREA MULCH	LYNGSO GARDEN MATERIALS	MOCHA CHIP	NATURAL	3" THICK LAYER

WALL LEGEND

WALL LEGEND					
LEGEND	MATERIALS	MODEL / MANUFACTURER	COLOR / FINISH		COMMENTS
W1	CUSTOM FLOW THROUGH PLANTER WALL	-	-	-	PER ARCHITECTURE
W2	CAST IN PLACE CUSTOM BENCH WITH STONE TOP	COLDSRINGUSA	SIERRA WHITE	LIGHT BROOM HONED	-
W3	CAST IN PLACE CUSTOM PLANTER WALL	-	NATURAL GRAY	TOPCAST MICRO-ETCH	MATCH WITH W1
W3A	CAST IN PLACE CUSTOM BENCH	-	NATURAL GRAY	TOPCAST MICRO-ETCH	MATCH WITH W1
W4	PERIMETER RETAINING WALL	-	-	-	SEE CIVIL/ ARCHITECTURE DRAWINGS

FURNITURE LEGEND

FURNITURE LEGEND					
LEGEND	MATERIALS	MODEL / MANUFACTURER	COLOR / FINISH		COMMENTS
S1	TABLES & CHAIRS	FORMS+SURFACES VISTA CHAIRS	BRIGHT SILVER	GLOSS POWDERCOAT	-
S2	BIKE RACK	-	STAINLESS #4 BRUSH FINISH	-	PER CITY STANDARD
S3	TRASH RECEPTACLES	LANDSCAPEFORMS FGP LITTER	WOOD ALUMINUM	POWDERCOAT	-
TG1	METAL TREE PIT EDGE	-	STAINLESS STEEL	-	-
G1	GATE AT NORTH PLAZA	-	-	-	SAD FOR DETAILS
G2	GATE AT BANCROFT WAY	-	-	-	SAD FOR DETAILS
F1	WEST FENCE	-	-	-	SAD FOR DETAILS

IRRIGATION LEGEND

IRRIGATION LEGEND				
LEGEND	NAME	MODEL / MANUFACTURER	COLOR	COMMENTS
	IRRIGATION POC COMPONENTS	TBD	TBD	-1" WATER METER, B.P.D., MASTER VALVE, FLOW SENSOR
	ELECTRIC CONTROLLER	-	-	-120 VOLT ELECTRICAL CONNECTION REQUIRED
	REMOTE CONTROL	TBD	TBD	-PROPOSED LOCATIONS
	IRRIGATION LATERAL LINE	-	-	-1120/SCHEDULE 40 PVC PIPE
	IRRIGATION SUPPLYLINE-1 1/2" & SMALLER	-	-	-1120/SCHEDULE 40 PVC PIPE
	SLEEVING-SIZE AS INDICATED	-	-	-1120/SCHEDULE 40 PVC PIPE
	L.V. ELECTRICAL CONDUIT-SIZE AS INDICATED	-	-	-1120/SCHEDULE 40 PVC PIPE
	PLANTING AREA LOW WATER USE	TBD	TBD	-
	PLANTING AREA MEDIUM WATER USE	-	-	-
	ROW PLANTER LOW WATER USE	TBD	TBD	-
	STORMWATER TREATMENT AREA LOW WATER USE	-	-	-

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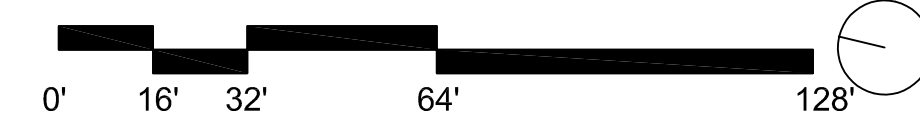
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2	USE PERMIT RESUBMITTAL	2021.09.13
3	USE PERMIT RESUBMITTAL	2021.12.17
4	1ST DESIGN REVIEW COMMITTEE MEETING	2022.02.04

Key Plan:

Seal & Signature:

NOT FOR CONSTRUCTION

Sheet Name:
LAB LANDSCAPE MASTER PLAN



LAB LANDSCAPE MASTER PLAN **01**
 SCALE: 1/32" = 1'-0"

Project No.:	220175	Sheet No.:	LP2.00.00
Drawn By:			
Checked By:			
Scale:	1/32" = 1'-0"		

787 BANCROFT

787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

PROJECT TEAM
 ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
SOM
 SKIDMORE, OWINGS & MERRILL LLP
 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

BUILDER
 LUSARDI CONSTRUCTION COMPANY
 6376 CLARK AVE
 DUBLIN, CA 94568

CIVIL ENGINEER
 WARE MALCOMB
 4683 CHABOT DRIVE #300
 PLEASANTON, CA 94588

MEP ENGINEER
 MEYERS+ ENGINEERS
 98 BATTERY STREET, SUITE 500
 SAN FRANCISCO, CA 94111

GEOTECHNICAL ENGINEER
 ROCKRIDGE GEOTECHNICAL
 270 GRAND AVE
 OAKLAND, CA 94610

SUSTAINABILITY
 ATELIER TEN
 443 TEHAMA ST., 1ST FLOOR
 SAN FRANCISCO, CA 94103

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Sheet Name:
LANDSCAPE PLAN

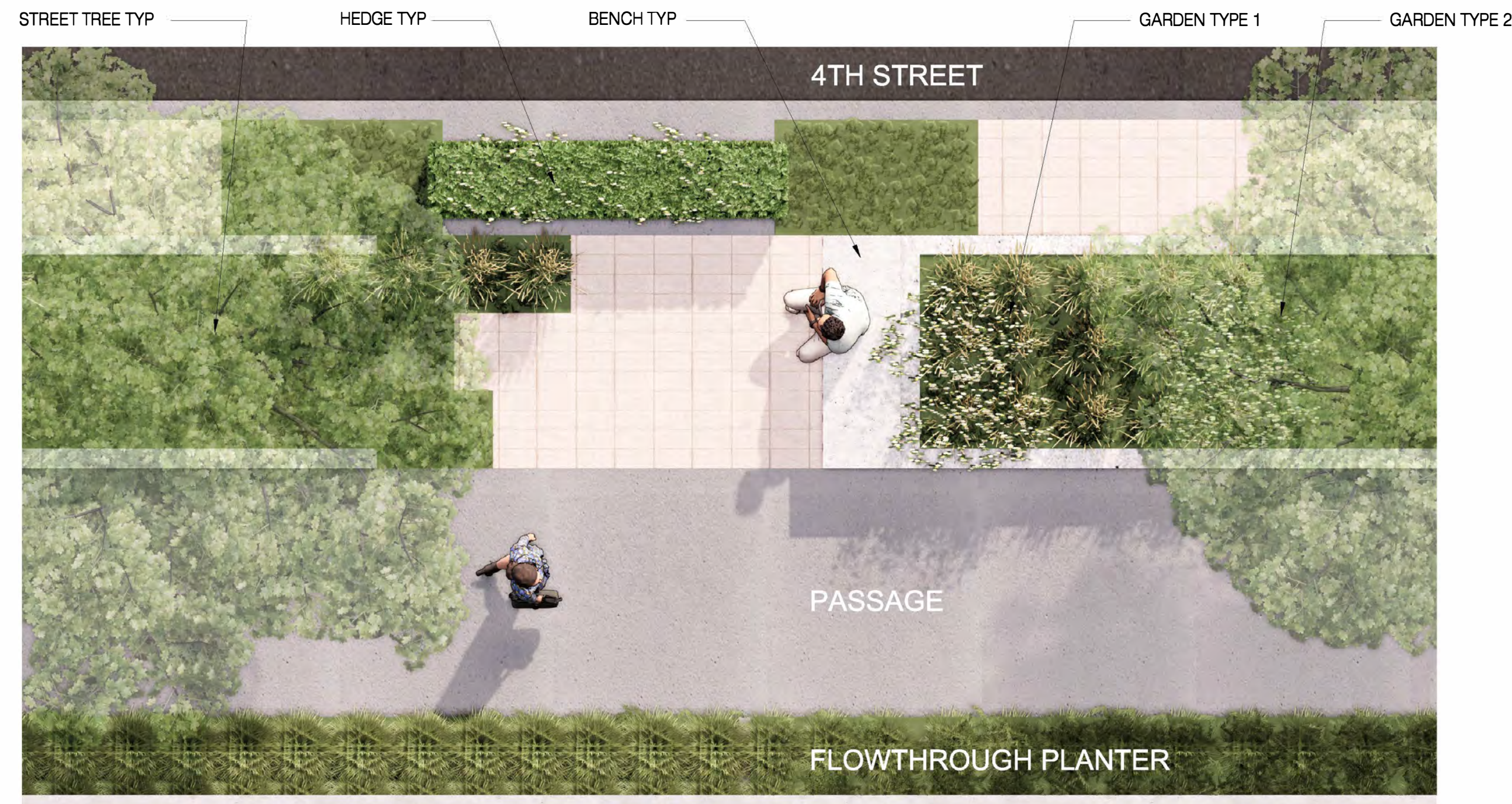
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 Sheet No.: **LP2.00.01**
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 Scale: 1/16" = 1'-0"



NOTES:
 1. SEE LEGEND ON SHEET LP1.00.00-LEGEND SCHEDULE

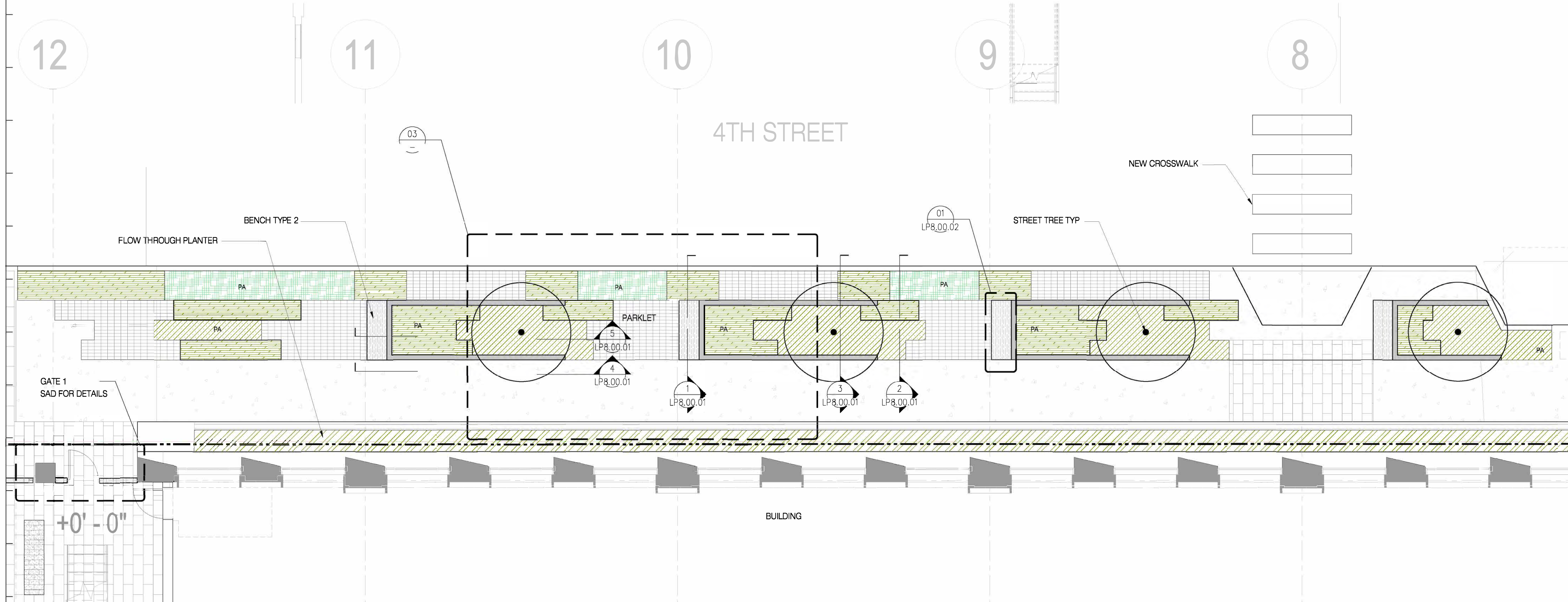
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NOTES:
 1. SEE SHEET LP5.01.00 AND LP5.01.01 FOR PLANTING.



SCALE: NTS
 STREET GARDEN RENDERED PLAN 03

SCALE: NTS
 STREET GARDEN RENDERING 02



SCALE: 1/4"=1'-0"
 STREET GARDEN ENLARGED PLAN 01

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787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

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SOM
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 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

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 LUSARDI CONSTRUCTION COMPANY
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 DUBLIN, CA 94568

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 ROCKRIDGE GEOTECHNICAL
 270 GRAND AVE
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Key Plan:

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Sheet Name:
ENLARGEMENT PLAN

Project No.: 220175	Sheet No.: LP2.00.11
Drawn By:	Checked By:
Scale: AS SHOWN	

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OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

PROJECT TEAM
 ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
SOM
 SKIDMORE, OWINGS & MERRILL LLP
 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

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 LUSARDI CONSTRUCTION COMPANY
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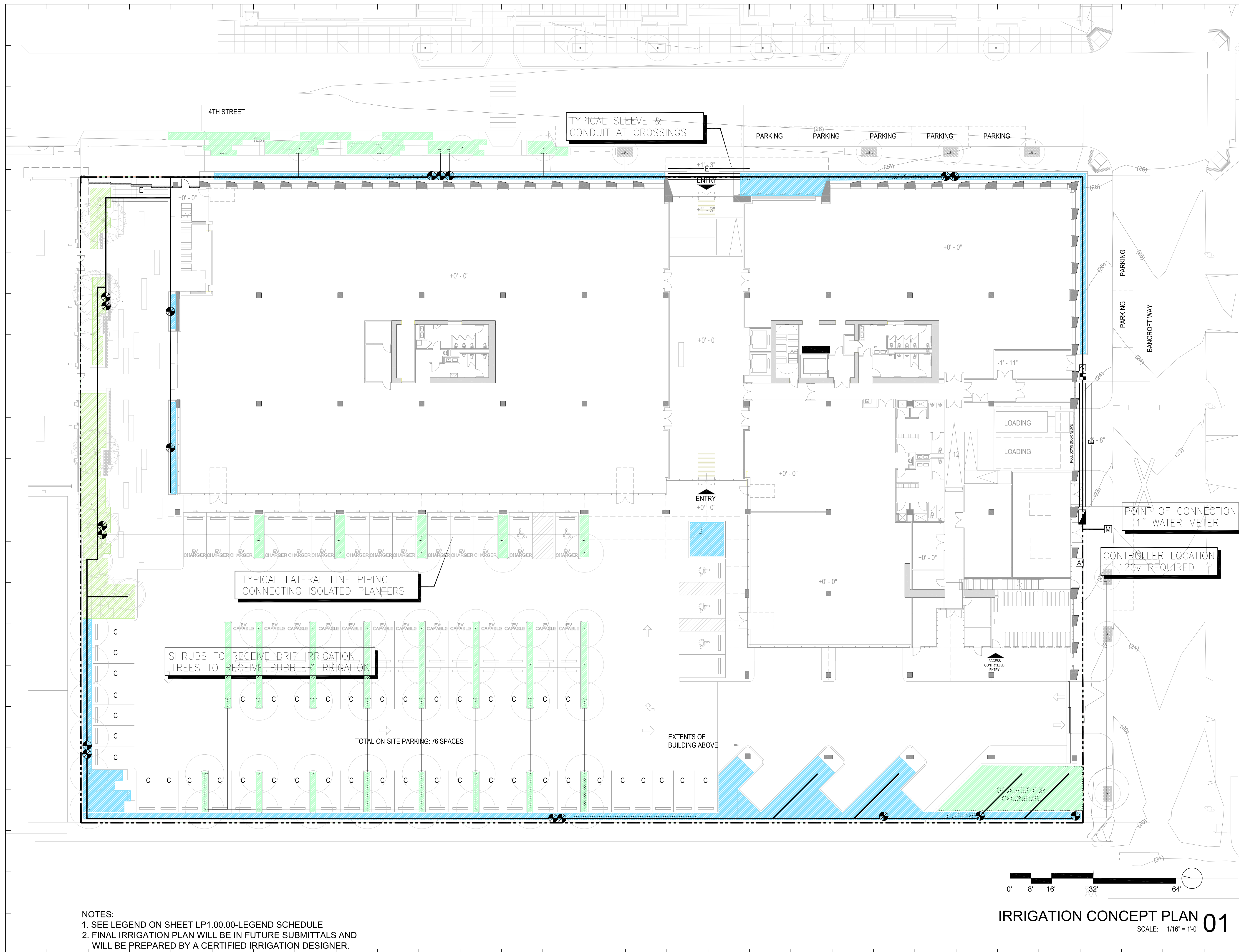
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Seal & Signature:

NOT FOR CONSTRUCTION

Sheet Name:
IRRIGATION CONCEPT PLAN

Project No.: 220175
 Sheet No.: **LP4.00.00**
 Drawn By:
 Checked By:
 Scale: 1/16" = 1'-0"



- NOTES:
 1. SEE LEGEND ON SHEET LP1.00.00-LEGEND SCHEDULE
 2. FINAL IRRIGATION PLAN WILL BE IN FUTURE SUBMITTALS AND WILL BE PREPARED BY A CERTIFIED IRRIGATION DESIGNER.

IRRIGATION CONCEPT PLAN 01
 SCALE: 1/16" = 1'-0"



PLAZA TREE
LONDON PLANE TREE
PLATANUS HYBRIDA



STREET TREE
RED MAPLE
ACER RUBRUM 'OCTOBER GLORY'



PARKING LOT TREE
FRONTIER ELM
ULMUS HYBRID 'FRONTIER'



PLAZA TREE
JAPANESE MAPLE
ACER JAPONICUM



STREET TREE
RED MAPLE
ACER RUBRUM 'OCTOBER GLORY'
FALL COLOR



PARKING LOT TREE
FRONTIER ELM
ULMUS HYBRID 'FRONTIER'
FALL COLOR

787 BANCROFT

787 BANCROFT, BERKELEY, CALIFORNIA

OWNER



STEELWAVE
101 CALIFORNIA STREET, SUITE 800
SAN FRANCISCO, CA 94111

PROJECT TEAM

ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE



SKIDMORE, OWINGS & MERRILL LLP
ONE MARITIME PLAZA
SAN FRANCISCO, CA 94111

BUILDER

LUSARDI CONSTRUCTION COMPANY
6376 CLARK AVE
DUBLIN, CA 94568

CIVIL ENGINEER

WARE MALCOMB
4683 CHABOT DRIVE #300
PLEASANTON, CA 94588

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98 BATTERY STREET, SUITE 500
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Key Plan:

Seal & Signature:

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Sheet Name:

PRECEDENT
IMAGES

Project No.:

Sheet No.:

220175

Drawn By:

Checked By:

Scale:

NTS

LP5.00.00



CAREX PANSA



LOMANDRA LONGIFOLIA 'BREEZE'



MUHLENBERGIA RIGENS



PENNISETUM ALOPECUROIDES



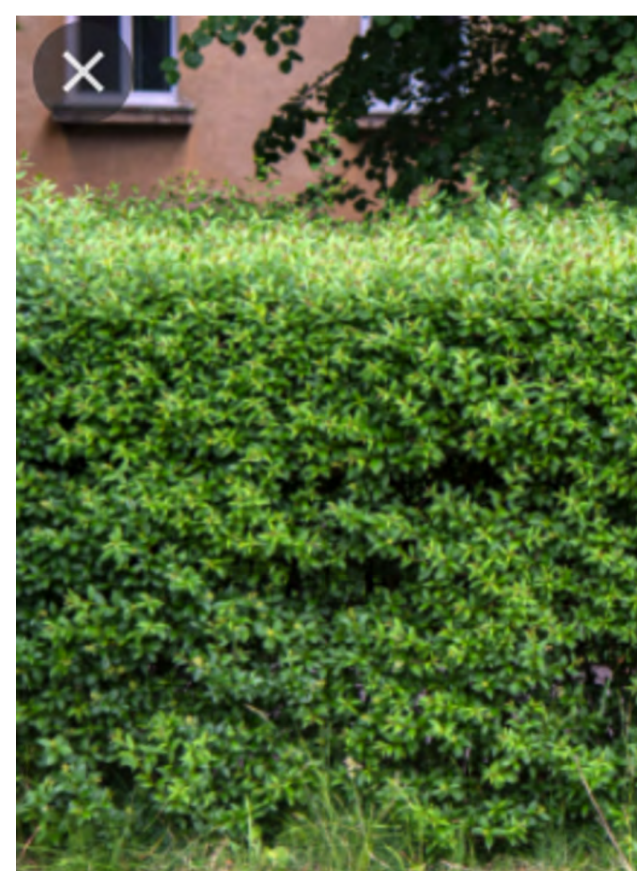
ARCTOSTAPHYLOS 'EMERALD CARPET'



CAREX TUMULICOLA



IRIS PACIFIC COAST HYBRID



LIGUSTRUM OVALIFOLIUM

787 BANCROFT

787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

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 ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
SOM
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 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

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 LUSARDI CONSTRUCTION COMPANY
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Key Plan:

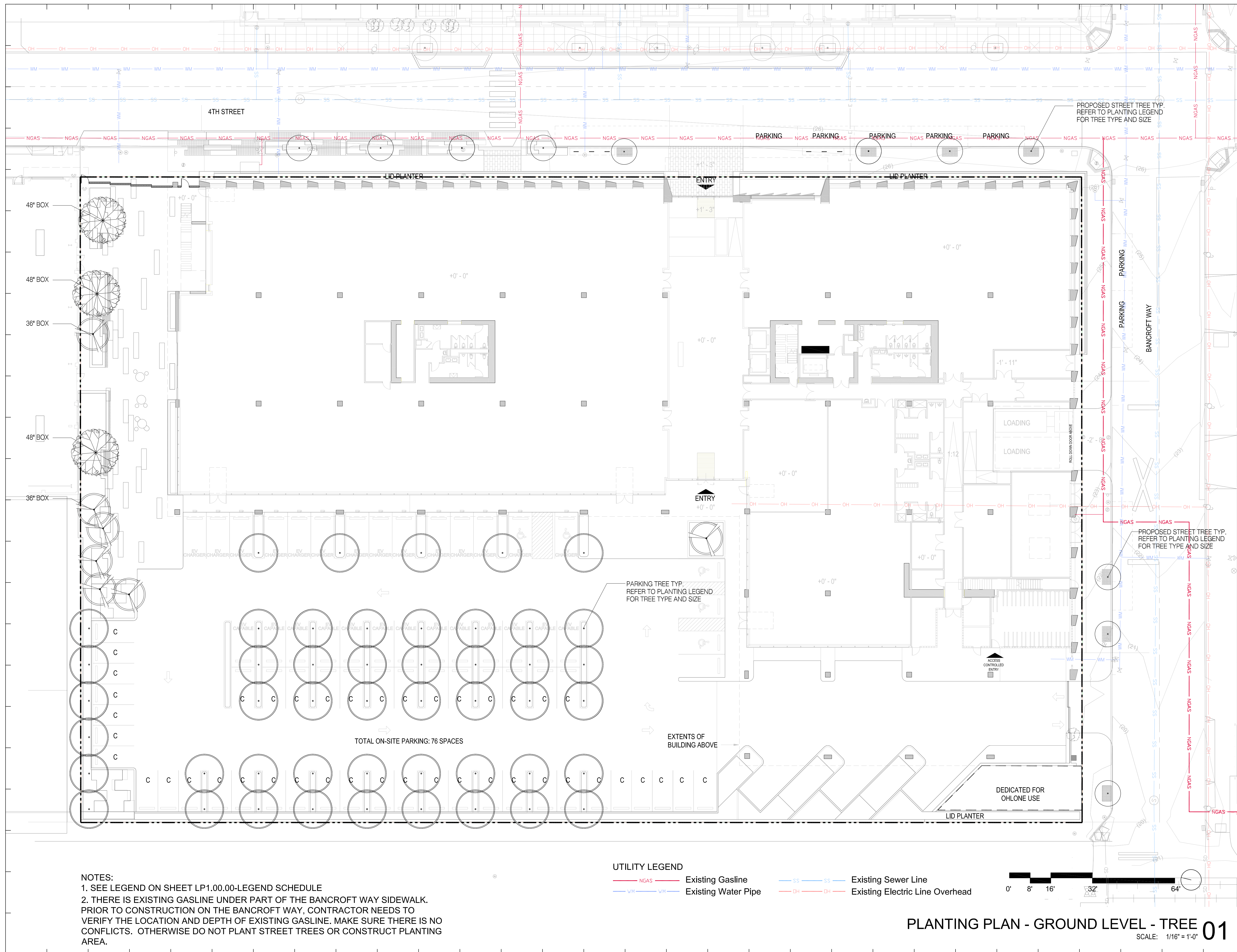
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Sheet Name:

PLANTING PLAN - GROUND LEVEL TREE

Project No.:	220175	Sheet No.:	LP5.01.00
Drawn By:		Checked By:	
Scale:	1/16" = 1'-0"		



NOTES:
 1. SEE LEGEND ON SHEET LP1.00.00-LEGEND SCHEDULE
 2. THERE IS EXISTING GASLINE UNDER PART OF THE BANCROFT WAY SIDEWALK. PRIOR TO CONSTRUCTION ON THE BANCROFT WAY, CONTRACTOR NEEDS TO VERIFY THE LOCATION AND DEPTH OF EXISTING GASLINE. MAKE SURE THERE IS NO CONFLICTS. OTHERWISE DO NOT PLANT STREET TREES OR CONSTRUCT PLANTING AREA.

UTILITY LEGEND

— NGAS — Existing Gasline	— SS — Existing Sewer Line
— WM — Existing Water Pipe	— DH — Existing Electric Line Overhead

PLANTING PLAN - GROUND LEVEL - TREE 01

SCALE: 1/16" = 1'-0"

787 BANCROFT
 787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
 STEELWAVE
 101 CALIFORNIA STREET, SUITE 800
 SAN FRANCISCO, CA 94111

PROJECT TEAM
 ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
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 SKIDMORE, OWINGS & MERRILL LLP
 ONE MARITIME PLAZA
 SAN FRANCISCO, CA 94111

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 LUSARDI CONSTRUCTION COMPANY
 6376 CLARK AVE
 DUBLIN, CA 94568

CIVIL ENGINEER
 WARE MALCOMB
 4653 CHABOT DRIVE #300
 PLEASANTON, CA 94588

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 90 BATTERY STREET, SUITE 500
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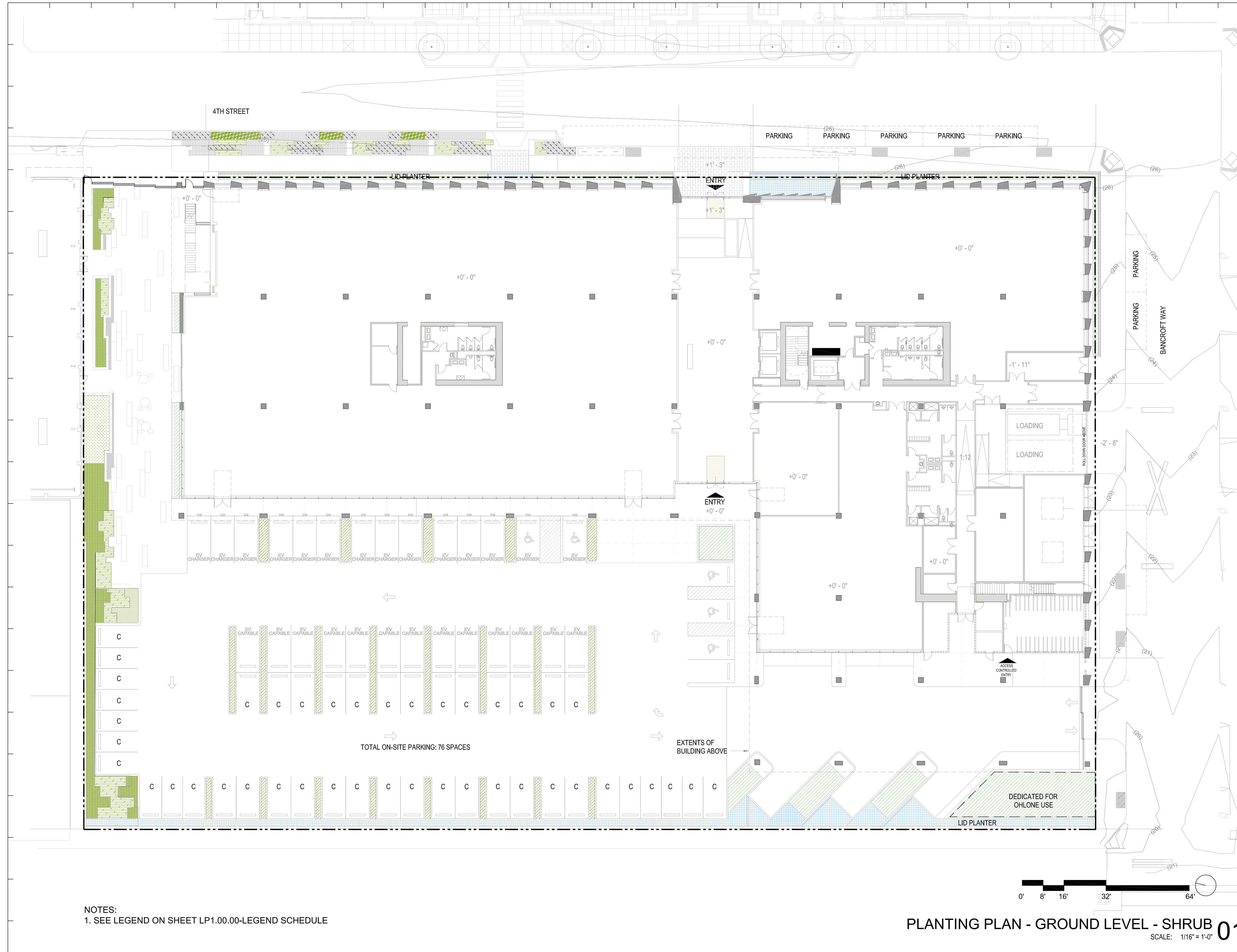
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Seal & Signature:

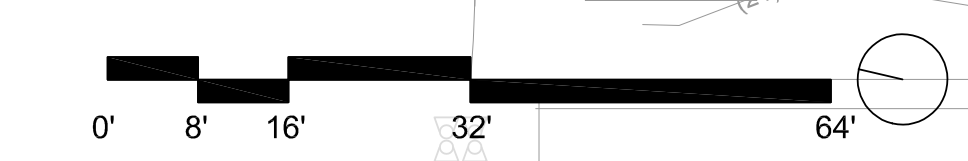
NOT FOR CONSTRUCTION

Sheet Name:
PLANTING PLAN - GROUND LEVEL SHRUB

Project No.: 220175	Sheet No.: LP5.01.01
Drawn By:	
Checked By:	
Scale: 1/16" = 1'-0"	



NOTES:
 1. SEE LEGEND ON SHEET LP1.00.00-LEGEND SCHEDULE



PLANTING PLAN - GROUND LEVEL - SHRUB 01
 SCALE: 1/16" = 1'-0"

787 BANCROFT

787 BANCROFT, BERKELEY, CALIFORNIA

OWNER
STEELWAVE
STEELWAVE
101 CALIFORNIA STREET, SUITE 800
SAN FRANCISCO, CA 94111

PROJECT TEAM
ARCHITECT | STRUCTURAL ENGINEER | LANDSCAPE
SOM
SKIDMORE, OWINGS & MERRILL LLP
ONE MARITIME PLAZA
SAN FRANCISCO, CA 94111

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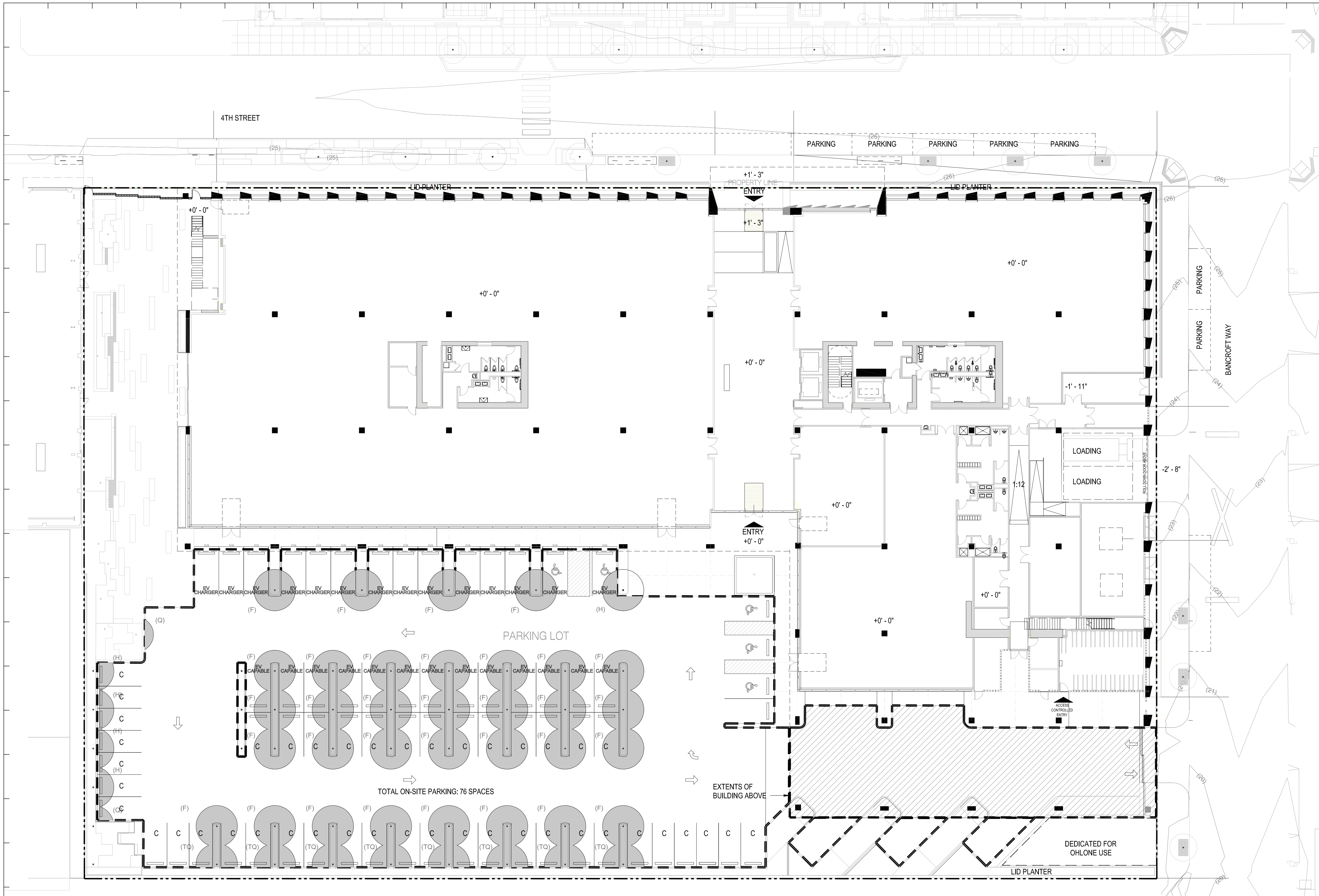
Key Plan:

Seal & Signature:

NOT FOR CONSTRUCTION

Sheet Name:
SHADED PARKING COVERAGE PLAN

Project No.:	220175
Sheet No.:	LP5.03.00
Drawn By:	
Checked By:	
Scale:	1/16" = 1'-0"



TOTAL OPEN AIR PAVED PARKING AREA	25,082 S.F.
TOTAL PARKING AREA SHADED BY TREE	7421.4 S.F.
TREE CANOPY SHADED PERCENTAGE	29.5%

(F)	100%	33 x 100' x 176.7 sqft = 5831.1
(TQ)	75%	8 x 75' x 176.7 sqft = 1060.2
(H)	50%	5 x 50' x 176.7 sqft = 441.75
(Q)	25%	2 x 25' x 176.7 sqft = 88.35
TOTALS 7421.4 sqft		

NOTES:
1. SEE LEGEND ON SHEET LP1.00.00-LEGEND SCHEDULE
2. BESIDES THE TREE CANOPY SHADED AREA, 4,326.0 S.F. OF PAVED SURFACE PARKING AND DRIVEWAY IS SHADED BY 787 BANCROFT.

SHADED PARKING COVERAGE PLAN 01
SCALE: 1/16" = 1'-0"

theLab

Phase III: West Block
Revised Applicant Statement
December 21, 2021

OVERVIEW

This applicant statement supports redevelopment of the west block of “theLab” campus, Phase III of a three-phase redevelopment project in West Berkeley. As the largest building within theLab, 787 Bancroft will contribute to the creation of a new hub of light manufacturing and research and development (R&D) uses in the neighborhood.

This project, and the previously-submitted Phase I and Phase II on the east block, will be tied together through landscaping, open space, and improvements to the public realm to create a new employment center in a campus-like environment along Fourth Street. A combination of increased open space, new construction, and enhancements to the public realm will improve the environment and support the development of businesses in the surrounding neighborhood.

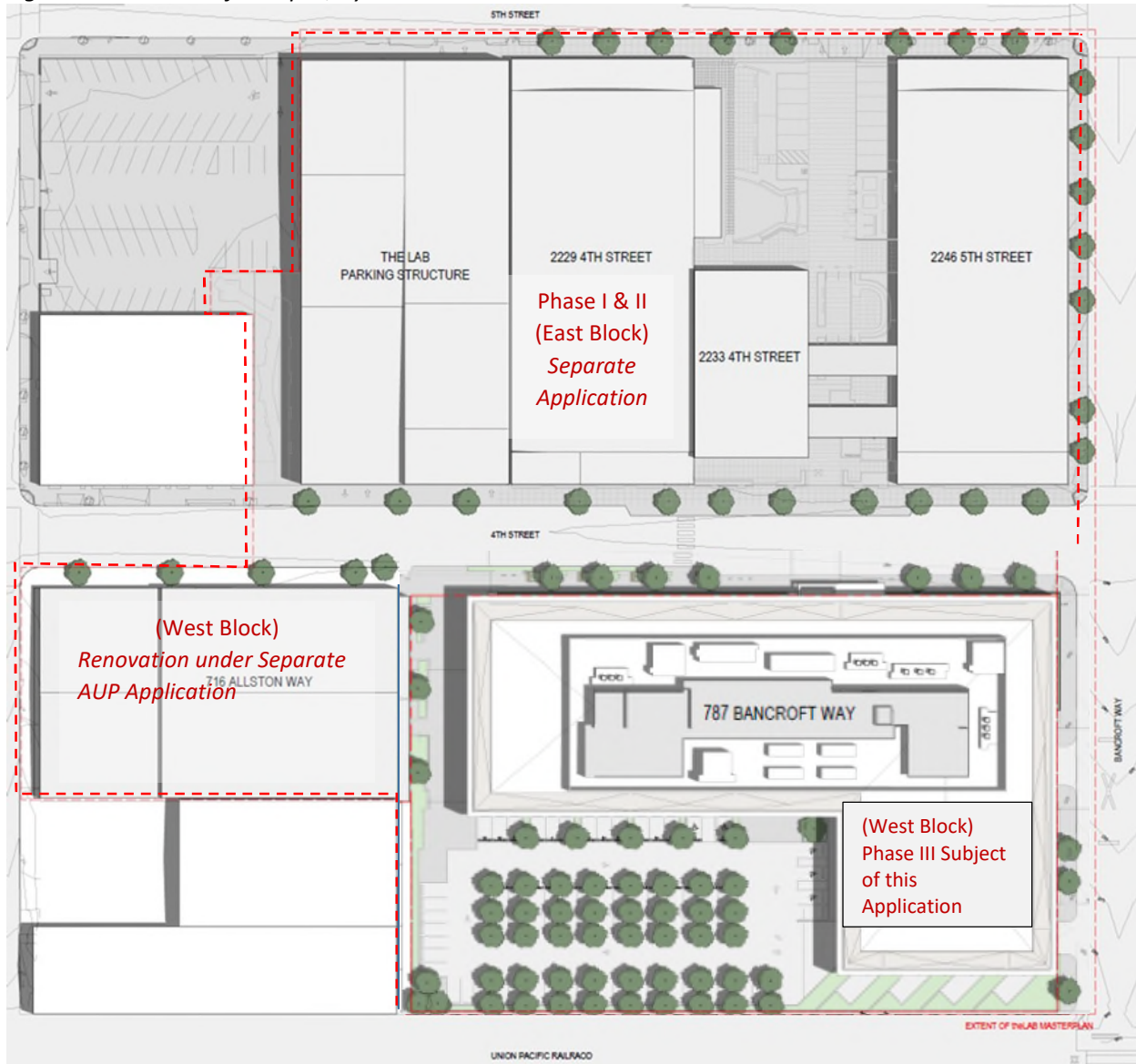
The three-phase project will redevelop the majority of the two blocks bounded by the Union Pacific Railroad (UPRR) tracks to the west, Allston Way to the north, Fifth Street to the east, and Bancroft Way to the south. Table 1 summarizes the proposals and status of each project phase. Phase III is the subject of this application, Phases I and II are separate applications. These areas are illustrated in Figure 1.



Table 1: Proposed Phases of theLab

<i>Phase</i>	<i>Proposal</i>	<i>Status</i>
Phase I & II (East Block)	<ul style="list-style-type: none"> • Tenant improvements (no use permits required) • Demolition of 4 Structures in the East Block • Construction of 415-stall parking garage 	Permit Under Review <i>Not further discussed in this application</i>
Phase III (West Block)	<ul style="list-style-type: none"> • Demolition of six small buildings in the West Block • Construction of 787 Bancroft: 3-story/159,143 sq. ft. light manufacturing and R&D space 	Proposed Herein <i>Subject of this application</i>
	?	

Figure 1: Overview of Campus, by Phase



Proposed 787 Bancroft Project

787 Bancroft would consist of 159,143 sq. ft. of light manufacturing and research & development space. The 3-story building would have frontages along Fourth Street and Bancroft Way. Parking is accommodated on-site in a surface lot and off-site, across the street, in the parking garage proposed as part of Phase II.

The building's L-shaped configuration and long horizontal proportions maintain the form and character of the surrounding blocks. With two major frontages, the building retains the street



walls on both Fourth Street and Bancroft Way while allowing for a generous public open space to the north of the building and public improvements along Fourth Street.

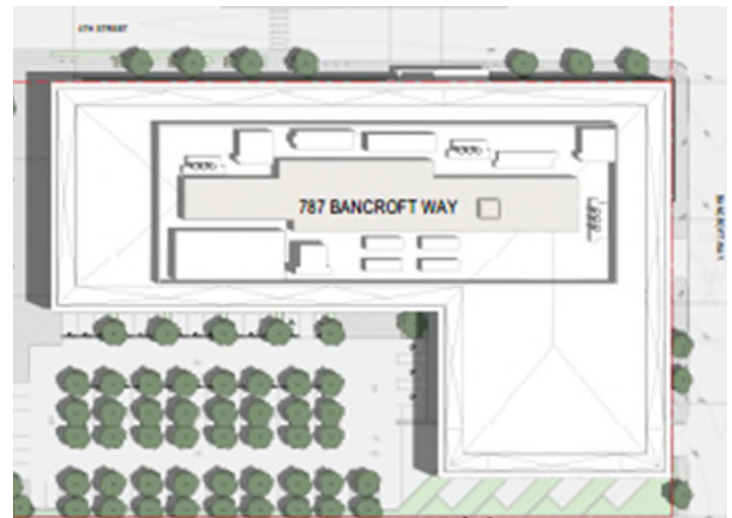
Under existing conditions there are limited setbacks between buildings, pedestrian movement is constrained, and there is limited pervious area and planting. The form and materiality of the building recall the simplicity and honesty of the industrial buildings in the neighborhood while making the block more welcoming and the pedestrian experience more enjoyable. A combination of concrete panels, wood soffits, glass, and metal trim complement the industrial character of the district, while providing some added warmth to the evolving neighborhood and its more pedestrian and bicycle orientation.

The ground floor is slightly recessed to increase the sidewalk width for pedestrian movement and additional planting and seating. The recessed ground level gives the building the same strong horizontal proportions as the previous building, despite being one story taller. The entry and address sign are near the midpoint of the Fourth Street façade, breaking up the long mass on the ground floor. The entry is aligned with a new open space across the street that ties all of the buildings of the Lab into a singular campus-like work environment.

The project is pursuing an all-electric design, consistent with the City's requirements, and per allowances in CEnC section 110.10(b)1B ("the solar zone shall be located on the roof or overhang of the building...located within 250 feet of the building or on covered parking installed with the building project and have a total area no less than 15 percent of the total roof area of the building excluding and skylight area.") the project will meet the photovoltaic requirement by installing solar panels on the top level of the parking garage, located at 2213 Fourth Street which is within 250 feet of the 787 Bancroft Way building. The proposed project will not include any natural gas infrastructure in compliance with BMC Chapter 12.80. The proposed project is designed to comply with the Berkeley Energy Code (BMC Chapter 19.36) and the Berkeley Green Code (BMC Chapter 19.37), including solar PV, electric vehicle charging, and low-carbon concrete requirements.

Proposed Removal of 703 Bancroft Way

- The original proposal for the 703 Bancroft structure, as described in the previous Applicant Statement, was to attempt to maintain the structure on the site and use it as a canopy over surface parking. After extensive study and comments received from the City's Building & Safety Division as well as the Berkeley Fire Department, it is no longer feasible to keep the 703 Bancroft Structure on site. Comments received at the staff interdepartmental roundtable meeting, after the submittal of the Use Permit application, as well as at a subsequent meeting with staff from both Building and Safety and the Berkeley Fire Department, resulted in the following issues that impacted the feasibility of maintaining the building on site: Adjacency of 703 Bancroft to the internal North and West property lines would require fire rated walls at the building openings. This would negatively impact maintaining the historic character of the building by blocking views into the structure.



- The placement of the 703 Bancroft building and its proximity to the proposed 787 Bancroft building would create challenging conditions for fire truck access & would mandate fire rated walls on levels 1-3 of the West façade of the 787 Bancroft building, minimalizing glazing and the architectural character of the new building.
- The 703 Bancroft building would not be counted towards shade canopy in the surface parking lot. Removal would allow for the addition of 20+ trees to the surface parking lot, reducing the heat island effect and furthering the City's Climate Action Plan goals.

Site Access and Parking

The pedestrian entrances to the site are located off Fourth Street. Vehicular access to the site is provided on Bancroft Way. This entry location reduces the curb cuts along Fourth Street to enhance the pedestrian experience and connectivity between buildings on either side of the street. A surface parking lot, with 75 spaces, is located on the interior of the parcel with access off of Bancroft Way.

Parking spaces to meet demand from the floor area proposed under this application will be provided in two locations: the 75-space surface lot described in the preceding paragraph and in a 415-space parking garage across Fourth Street (east block, under previous Phase II application) that serves the entire development.¹ This adjacent parcel is under the same ownership as the subject parcel and within 300 feet of the project site; as such, this off-site parking is permitted by right under BMC 23E.28.030.A. The MU-LI parking requirements per BMC Section 23E.80.080 will be met for each project phase, as summarized in Table 2.

Table 2: Summary of Proposed Parking, by Phase

Phase	Floor Area ³	Parking Ratios (BMC 23E.80.080)	Parking, by Use	
			Required	Provided
I & II (East Block) ¹	74,812	Manufacturing: 1 sp./1,000 sf (<10,000 sf); 1 space/1,500 sq. ft. (10,000+ sf) R&D: 2 sp./1,000 sf	63	147
III (West Block) ³	159,143		274	294
Total	233,955		337	441

¹ For informational purposes; not part of this application.

³ Includes gross floor area for R&D and Light Manufacturing uses.

Project Benefits

The project includes the following benefits to the West Berkeley community and the City of Berkeley:

- **Public Improvements:** A mid-block crosswalk improves walkability close to the Fourth Street retail corridor. A new public parklet and street trees provide opportunities for shade and public open space along Fourth Street. Extensive and coordinated streetscape improvements along both sides of Fourth Street, and on Bancroft Way. This will extend pedestrian improvements to the UPRR tracks which will connect with new pedestrian sidewalk access being built by the Berkeley Commons/600 Addison Street project. This will enable pedestrians to walk on the north side of Bancroft Way all the way to the Aquatic Park, which is not currently possible.

- **Public Art:** The project proposes to pay a fee in-lieu of \$164,905 to meet the Public Art Program requirement. The project sponsor typically locates additional public art on its project sites.
- **Green Building:** Photovoltaic solar panels on the top level of the parking garage located at 2213 Fourth Street, meeting the Berkeley REACH Code requirement for PV on 15% of the equivalent rooftop area of the new 787 Bancroft building , secure bike parking room (and showers) located on the ground floor of 787 Bancroft, and minimum of LEED Silver or equivalent.
- **Water Quality and Stormwater:** Bioretention systems will slow and clean stormwater replacing the existing impervious lot which does not manage or clean stormwater before it enters the Aquatic Park lagoon and San Francisco Bay. Drought-tolerant landscape plantings reduce water use and helps to sequester carbon.
- **Job Opportunities:** Job opportunities at a range of qualification levels as part of the light manufacturing and research & development campus, and short-term jobs during construction.
- **Affordable Housing, Childcare, and City Revenues:** The project would contribute over \$1.2 million to the Berkeley Unified School District and the City of Berkeley in the Affordable Housing Trust Fund and Child Care Mitigation Fees.

SITE CONTEXT AND ZONING CONFORMANCE

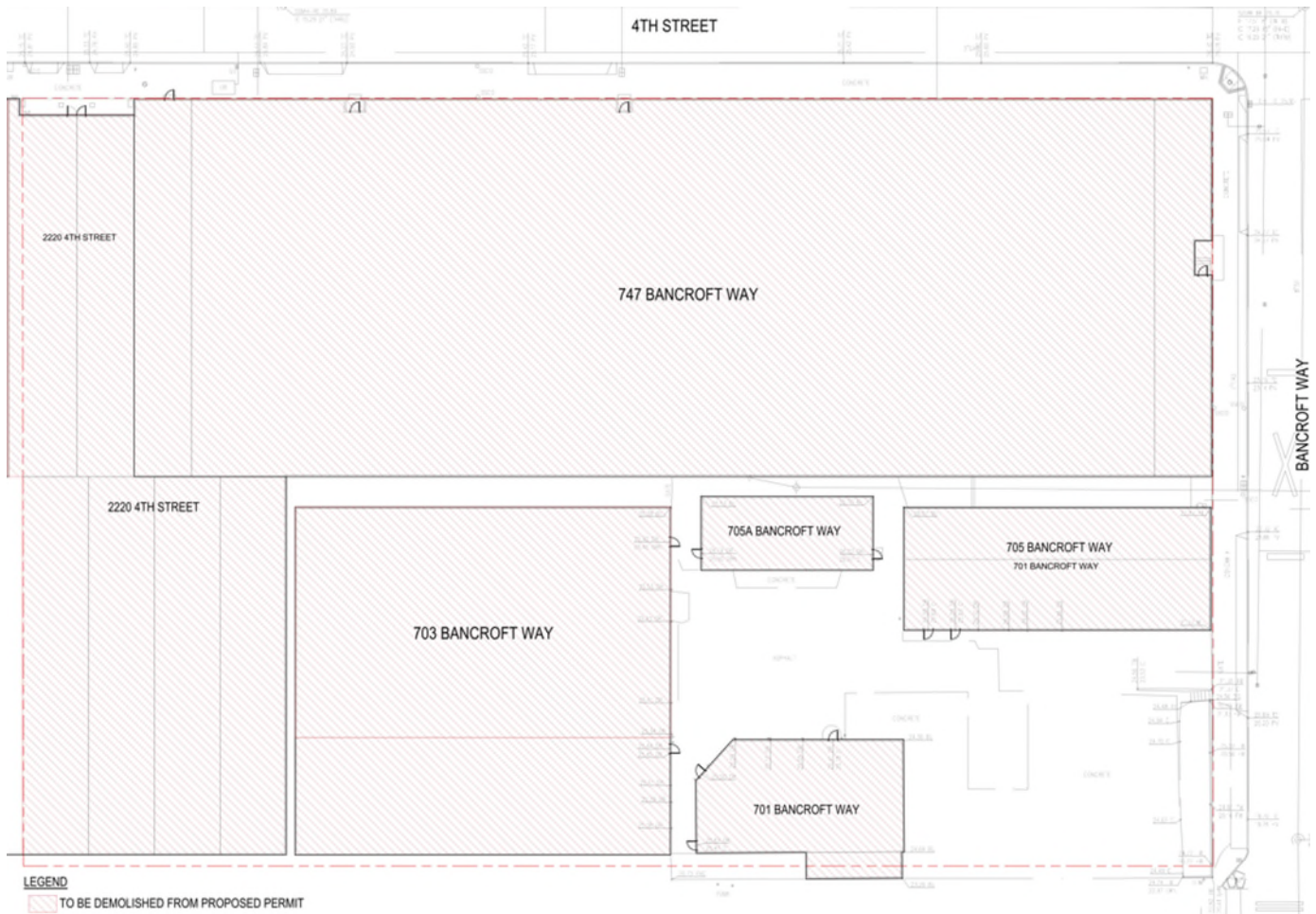
The project site is located in West Berkeley and is surrounded by the UPRR tracks to the west, Fourth Street to the east, and Bancroft Way to the south. The 96,966-sq. ft. parcel is designated as Manufacturing (M) in the General Plan and Mixed Use Light Industrial (MU-LI) in the Zoning Ordinance and the West Berkeley Area Plan. The site is surrounded by manufacturing and warehouse uses.

The project site is composed of three separate parcels which will be realigned to create two parcels through a lot line adjustment. The parcel is currently occupied by surface parking and six buildings, six of which are proposed to be demolished as part of this application, as shown in Table 3 and Figure 2.

Table 3: Proposal for Existing Buildings on Subject Property

<i>Building Address</i>	<i>Status</i>
747 Bancroft Way	Demolish
2220 4th Street	Demolish
701 Bancroft Way	Demolish
705 Bancroft Way	Demolish
705A Bancroft Way	Demolish
703 Bancroft Way	Demolish

Figure 2: Demolition Plan



Buildings Proposed for Demolition

The buildings proposed for demolition have come to the end of their useful life and cannot be repurposed for the type of R&D and light manufacturing expected at the project site. The demolition of buildings at 2220 4th Street, 747, 701, 703, 705 and 705 A Bancroft Way will allow for construction of the proposed new 787 Bancroft building and its related improvements and surface parking lot.

Protected Uses

Both the project site and the full extent of the three-phase project, include uses that are classified as “protected” under BMC Section 23E.80. This includes existing uses devoted to warehouse, light manufacturing, warehouse-based retail, which are required to be replaced on a 1:1 or lower basis depending on the proposed new use.

The project team has worked with City staff to determine the requirements for protected uses across the entire three-phase project. Upon completion of both phases, the project will replace all required protected uses on-site. As shown, this includes a total of 112,274. ft. of proposed protected uses across both phases. This includes protected use area proposed as part of this Phase III application: 33,347 sq. ft. devoted to light manufacturing uses in 787 Bancroft. In order to meet the letter of the City’s zoning regulations, which require replacement at the time of demolition, the project is requesting a temporary variance from the protected use requirement during the course of construction to allow the new protected floor area to be built and immediately replaced upon occupancy.

The plan set includes floor plans that indicate where this light manufacturing space will be located at time of initial lease up. Our team is requesting flexibility in where in the building the protected light manufacturing space is located at the initial lease up of the new building. As tenant demands change over time, more flexibility may be needed, but the project team understands that any such changes after initial lease up and occupancy may require additional or modified discretionary application per BMC Section 23E.80.045.B and C.

Mobility and Alternative Transportation

The project site is a multimodal hub and enjoys close access to waterfront recreation at Aquatic Park and shopping and dining on Fourth Street. The Berkeley Amtrak station is located two blocks to the north. The AC Transit 51 bus line stops two blocks away, with service to Downtown Berkeley, and the 80 line and West Berkeley Shuttle stop two blocks away, with service to the Ashby and El Cerrito Plaza BART stations. Additionally, the Transbay Z line provides critical service from San Francisco to Sixth Street during morning commute hours and back to San Francisco during afternoon commute hours.

The project proposes a Transportation Demand Management (TDM) program to encourage alternative modes of transportation:

- A. Provide shuttle service between the project and a BART Station during weekday peak commute periods (6:00 AM to 10:00 AM and 3:00 PM to 7:00 PM). The project would also explore the feasibility, and if feasible, will coordinate the shuttle service with existing shuttle services, and/or other employers in West Berkeley. Shuttle service would be adjusted based on ridership.
- B. Provide bike lockers, showers, personal lockers, and a repair station on-site to encourage bicycling to the site.
- C. Coordinate with City of Berkeley, and/or other regional agencies to facilitate the potential installation of a BayWheels bikeshare station along the project frontage.
- D. Offer carpool/ride-matching services, such as ZimRide, ComoVee, or 511.org RideShare, to pair employees interested in forming commute carpools.
- E. Provide at least 10 spaces of preferential carpool parking, including free parking for carpoolers if employees are charged for on-site parking. Carpool parking spaces not occupied by 10:00 AM would be available to other vehicles.
- F. Require tenants to provide full or partial transit subsidy to project employees. Tenants may offer one of the following to employees that request it:⁵
 - A monthly commuter check (or alternatively Clipper Card, which is accepted by BART, AC Transit, and other major transit providers in the Bay Area)
 - Subsidized AC Transit bus pass
 - Subsidized Capital Corridor monthly ticket
- G. Require tenants to provide pre-tax commuter benefits for project employees.
- H. Regularly provide project tenants and employees information about various transportation options in the area and the TDM strategies provided by the project. The main lobby of each major project building shall also provide all the information on transportation options, such as a Transit Screen.
- I. Provide information on the Bay Area Commuter Benefits Program to all building tenants. As of September 30, 2014, Bay Area employers with 50 or more full-time employees within the Bay Area Air Quality Management District (Air District) geographic boundaries are required to register and offer commuter benefits to their employees in order to comply with Air District

Regulation 14, Rule 1, also known as the Bay Area Commuter Benefits Program. Employers must select one of four Commuter Benefit options to offer their employees: a pre-tax benefit, an employer-provided subsidy, employer-provided transit, or an alternative commute benefit. (Information about Commute Benefits Program is at 511.org/employers/commuter/overview.)

Permits Requested

The application includes the following permits, including a request for approximately 133,028 sq. ft. for Research & Development use and approximately 29,347 sq. ft. for Light Manufacturing use:

1. **BMC 23C.08.050:** Use Permit to demolish six existing non-residential buildings (747 Bancroft Way, 2220 4th Street, 701 Bancroft Way, 703 Bancroft Way, 705 Bancroft Way, 705A Bancroft Way)
2. **BMC 23E.04.020:** Administrative Use Permit to allow rooftop equipment projections to exceed the height limit in a non-residential district;
3. **BMC 23E.80.030:** Administrative Use Permit to establish more than 30,000 SF R&D use;
4. **BMC 23E.80.045.B:** Use permit to change more than 25% of manufacturing and warehouse uses to R&D uses.
5. **BMC 23E.80.050.C:** Use Permit to construct between 20,000 and 30,000 square feet of new floor area as Manufacturing and Wholesale Trade, as per BMC Table 23E.80.030;
6. **BMC 23B.44.010:** Variance from BMC 23E.80.090.D.1., the protected use requirement, to allow replacement of protected floor area to be made available after the demolition or change of use of the space.

FINDINGS

23B.32.040 Findings for Issuance and Denial and Conditions

- A. *The Board may approve an application for a Use Permit, either as submitted or as modified, only upon finding that the establishment, maintenance or operation of the use, or the construction of a building, structure or addition thereto, under the circumstances of the particular case existing at the time at which the application is granted, will not be detrimental to the health, safety, peace, morals, comfort or general welfare of persons residing or working in the area or neighborhood of such proposed use or be detrimental or injurious to property and improvements of the adjacent properties, the surrounding area or neighborhood or to the general welfare of the City.*
- B. *Prior to approving any Use Permit the Board must also make any other findings required by either the general or District regulations applicable to that particular Use Permit.*
- C. *The Board shall deny an application for a Use Permit if it determines that it is unable to make any of the required findings, in which case it shall state the reasons for that determination.*
- D. *The Board may attach such conditions to any Use Permit as it deems reasonable or necessary to achieve the purposes of this Ordinance, and which otherwise promote the municipal health, safety and welfare.*

Response: The proposed project will neither be a detriment to the neighborhood nor to the City of Berkeley in general. The proposed project represents an improvement on an underutilized lot, which includes dated buildings and infrastructure. The project will provide top-of-the-line employment space, in a building with sustainability features that reduce greenhouse gas emissions, noise, traffic, and air quality impacts for both its neighbors and for the region. The new uses will generate job opportunities

at a range of qualification levels.

The project will also rebuild the curb, sidewalk, and gutter along Fourth Street and add bioretention systems to slow and clean stormwater, improving water quality in the Aquatic Park lagoon and San Francisco Bay, compared to existing conditions at the site. It will plant new trees and landscaping to reduce the heat island effect and create a more inviting streetscape on Fourth Street and Bancroft Way.

The project, therefore, aligns with Berkeley's General Plan, West Berkeley Plan, and Climate Action Plan goals which seek to increase employment opportunities and reduce greenhouse gas emissions.

23C.08.050 Demolitions of Buildings Used for Commercial, Manufacturing or Community, Institutional or Other Non-residential Uses

D. A Use Permit or an AUP for demolition of a non-residential building or structure may be approved only if the Board or Zoning Officer finds that the demolition will not be materially detrimental to the commercial needs and public interest of any affected neighborhood or the City, and one of the following findings that the demolition:

- 1. Is required to allow a proposed new building or other proposed new Use;*
- 2. Will remove a building which is unusable for activities which are compatible with the purposes of the District in which it is located or which is infeasible to modify for such uses;*
- 3. Will remove a structure which represents an unabatable attractive nuisance to the public; or*
- 4. Is required for the furtherance of specific plans or projects sponsored by the City or other local district or authority. In such cases, it shall be demonstrated that it is infeasible to obtain prior or concurrent approval for the new construction or new use which is contemplated by such specific plans or projects and that adhering to such a requirement would threaten the viability of the plan or project.*

Response: In response to finding 23C.08.050.D and 23C.08.050.D.1, the proposed project will not be materially detrimental to the commercial needs and public interest of the West Berkeley neighborhood. The demolition of 747 Bancroft Way, 2220 4th Street, 701 Bancroft Way, 703 Bancroft Way, 705 Bancroft Way, and 705A Bancroft Way is required for construction of a new building and related circulation and site improvements which will revitalize the neighborhood, expand the floor area dedicated to light manufacturing uses, and facilitate the growth of floor area dedicated to R&D uses.

The demolition will not remove buildings of architectural significance. The demolition will remove six underutilized buildings that do not meet industry standards for productive research and development uses, which have evolved in recent years. Several of these buildings have been vacant for a substantial amount of time, do not provide any substantial benefits to the neighborhood or the City that are not outweighed by the benefits of the proposed project as a whole.

The proposed project will expand the light manufacturing floor area in the MU-LI and MU-R Districts, and the construction of 787 Bancroft Way will revitalize and rehabilitate this portion of West Berkeley. It will provide infill development in a manner that is compatible with the existing City character, nearby land uses and architectural scale and design. The removal of existing structures will facilitate the removal of any necessary contaminated soil and groundwater at the site and the ultimate reuse of the site in a manner that will allow the site to be economically productive for the City of Berkeley in the future.

23E.80.090.A&B Findings for MU-LI District (New Uses/Structures)

- A. In order to approve any Use Permit under this chapter the Zoning Officer or Board must make the finding required by Section 23B.32.040. The Zoning Officer or Board must also make the*

findings required by the following paragraphs of this section to the extent applicable:

B. A proposed use or structure must:

- 1. Be consistent with the purposes of the District;*
- 2. Be compatible with the surrounding uses and buildings;*
- 3. Be consistent with the adopted West Berkeley Plan;*
- 4. Be unlikely, under reasonably foreseeable circumstances, to either induce a substantial change of use in buildings in the District from manufacturing, wholesale trade or warehousing uses;*
- 5. Be designed in such a manner to be supportive of the light industrial character of the district. Such physical compatibility shall include materials used; facade treatments; landscaping; lighting; type, size and placement of awnings, windows and signs; and all other externally visible aspects of the design of the building and site. If the building and/or site is split between the MU-LI District and the West Berkeley Commercial District that there are clear and appropriate distinctions in all design aspects between the portions of the building and site within the MU-LI District and the portions within the West Berkeley Commercial District*
- 6. Be able to meet any applicable performance standards as described in Section 23E.80.070.D.*

Response: The proposed uses and buildings are compatible with the purposes of the district in that the project will: develop R&D and light manufacturing uses that meet the West Berkeley Plan's designation of a Light Manufacturing District; create compatible uses within the MU-LI district; provide a range of job opportunities, including those that do not require advanced degrees; provide large floor plates and tall ceiling heights that would allow for medium- and large-sized companies and needs; provide opportunity for R&D space in an appropriate location and structure; and maintain and improve the quality of the West Berkeley environment through improvements to the quality of building materials, bioretention systems to improve stormwater quality, new site landscaping and infrastructure, and improvements to the public realm, including new sidewalks, a mid-block crosswalk, public parklet and landscaping. The replacement of underutilized structures with a new building dedicated to R&D and light manufacturing uses serves the purpose of fostering growth of advanced technology services, while encouraging the availability of buildings for manufacturing uses and jobs.

The proposed uses are compatible with the surrounding uses, which include warehousing, manufacturing, office, R&D, and laboratories. Additionally, the combination of light manufacturing and R&D uses within the subject site are inherently consistent with the district purpose to provide opportunities for office that will not interfere with light manufacturing uses or building stock. The project would further contribute to the economic and land use diversity described in the district purposes.

The proposed use is consistent with the West Berkeley Plan because it is a development that accommodates R&D and light manufacturing uses that maintain the mix of uses and economic diversity which gives West Berkeley its unique character. The West Berkeley Plan supports the start-up of new types of economic activity which creates opportunities for land and business development. Additionally, the proposed project would increase the number of employees on-site, expanding job opportunities, in line with the plan goals. Use of glass, concrete, and metal trim express the industrial nature of the building. With the addition of wood soffits, these materials are used in a more contemporary way than surrounding industrial structures, which are generally older and lack fenestration. Still, they complement the metal, steel, and concrete found on neighboring industrial buildings.

The proposed use will not create substantial dust, glare, noise, odor, vibration, hazardous materials, or any other potential off-site environmental impacts because it will be required to comply with performance standards applicable in West Berkeley. Deep overhangs and awnings reduce the possibility for glare. During operation of the project, mechanical systems propose to reduce dust, noise, odors, and

hazardous materials compared to existing conditions.

Policy Consistency

The proposed project fulfills key land use goals and policies of the West Berkeley Plan:

- Provides for continued economic use of manufacturing and R&D uses that will benefit potential workers who reside in the City of Berkeley, existing retail and restaurant businesses in West
- Berkeley through incidental shopping, and the industrial character of the area with a new state-of-the-art building. (Goal 1, Policy D)
- Expands employment opportunities at a range of education and qualification levels. (Goal 1, Policy A)
- Develops a sustainable building, site, and landscape plan with low-impact air quality, GHG, and noise impacts that is appropriate for the interface between the MU-LI and the nearby C-W district to the north (Goal 3)
- Expands the overall amount of light manufacturing space and potential life sciences industries in the MU-LI district by redeveloping an underutilized site, increasing the amount of floor area. (Goal 2, Policy B)
- Allows a wide range of light manufacturers to continue to operate and expand and limits loss of their spaces to other uses, while providing an opportunity for office development where it will not unduly interfere with light manufacturing uses, and for laboratory development in appropriate locations. (Goal 2, Policy C)
- Allows development that is consistent with all development standards requirements and therefore of a scale that is appropriate for the MU-LI district and its surroundings, which include 1 and 2-story buildings to the north, east, and south, and a pending 3-story buildings to the west. (Goal 4)
- Fosters the growth of advanced technology manufacturing and advanced technology services (such as research laboratories) by providing space for such enterprises. (Economic Development, Goal 4)
- Provides parking both to support the expansion of floor are dedicated to light industrial uses and creates a consolidated parking lot that would allow different uses to share parking in a consolidated location, while integrating the structure and lot with the surrounding areas. (Physical Form Goal 2, Policies 2.3 and 2.5)
- Creates and maintains adequate parking to support West Berkeley land use without creating increased incentives for single occupant automobile use. (Transportation Goal 4)

The project also supports the following goals of the Berkeley General Plan:

- Policy LU-3 –Infill Development: The project is an infill development that is architecturally and environmentally sensitive and is compatible with neighboring land uses and architectural design and scale.
- Policy LU-34 –Industrial Protection–The project protects industrial uses by expanding the existing space dedicated to industrial and manufacturing uses.
- Policy T-32 –Shared Parking–The parking component of the site-wide project makes the most efficient use of existing and new parking areas and provides parking that can be shared with locations throughout the site.

23E.80.090.D Findings for MU-LI District (Protected Uses)

- D. Except as permitted under 23E.80.045, subdivisions A.1 or A.2, in order to approve a Use Permit under Section 23E.80.045 to change the use of or remove more than 25% of the floor area of a building currently or most recently used for manufacturing, wholesale trade or warehousing, the Zoning Officer or Board must find:
1. Any necessary Use Permits that have been approved to provide comparable quality replacement manufacturing, wholesale trade and/or warehousing space in Berkeley at a comparable rent and that such replacement space will be available before the demolition or change of use of the space; or
 2. As a result of lawful business and building activities, there are exceptional physical circumstances (exclusive of the presence of hazardous materials in the building(s), soil or groundwater) found at the building not generally found in industrial buildings in the District which make it financially infeasible to reuse the building for any of the range of manufacturing, wholesale trade or warehouse uses permitted in the District. The analysis of the financial feasibility effects (which shall be verified by the City) of these physical circumstances shall consider those costs necessary to make the building meet current minimum standards for manufacturing, wholesale trade or warehouse buildings; and
 3. Appropriate mitigation has been made for loss of the manufacturing, warehousing or wholesale trade space in excess of 25% of that space through providing such space elsewhere in the City, payment into the West Berkeley Building Acquisition Fund, or by other appropriate means.

Request:

The project is requesting a variance from the protected use requirement per BMC Section BMC 23E.80.090.D.1 for Phase III of the project. The project sponsor is committed to providing light manufacturing space to meet the use permit requirements for protected uses on-site at full completion of the project. However, the project sponsor cannot physically replace the protected floor area such that "replacement space will be available before the demolition". This is an impossible threshold. As a result, the project sponsor is requesting a variance under subsection D.1. Subsection D.2 and D.3 are not applicable to the project since the existing warehouse and light manufacturing buildings are currently utilized. This variance is only necessary for a temporary period of approximately two years during the Phase III project's construction phase. Once Phase III is complete (i.e., at the time of certificate of occupancy), the project will include the protected floor area within the project. Notably, the program proposed under Phase I results in a surplus of protected floor area and therefore does not require a variance.

Findings for a Variance from BMC Section 23E.80.090.D.1., Protected Uses, During Construction

1. There are exceptional or extraordinary circumstances or conditions applying to the land, building or use referred to in the application, which circumstances or conditions do not apply generally to land, buildings and/or uses in the same District;

Response: Requiring the retention of protected uses on-site during the course of construction makes the project infeasible. There are multiple issues associated with the project site which in combination make it infeasible to phase grading and site preparation which could otherwise possibly allow for

retention of protected area during construction.

The project sponsor has designed Phase I to include change of use requests and the addition of mezzanines in buildings that increase the amount of manufacturing (i.e., protected floor area) during the first phase. However, in order to build the 787 Bancroft Way project in Phase III, the project needs to demolish the buildings within and immediately outside the new building footprint and build the parking that is required for the additional floor area. This results in the demolition of the protected floor area prior to replacement on-site which necessitate the variance request.

Large Multi-Use Site - The project site is one of the largest development sites in West Berkeley at 5.5 acres (east and west block combined). Across both phases, there are 13 existing buildings containing protected uses. Within these buildings, the City has identified a replacement requirement of 138,382 sq. ft. of protected floor area. There will be 7 structures across the two blocks at project completion. This lends a complexity to the site atypical in other West Berkeley locations. Because of the expansion of floor area (still below the City's 2.0 FAR limit) and related required parking spaces, the project needs to stagger the development of buildings and parking areas to serve those buildings.

Grading Program - Grading the entire west block at one time is the only feasible option for site preparation activities. If the site were phased for the purposes of retaining the existing buildings protected uses during construction, the project team would need to both off-haul and import dirt at different points in time. Phased grading would contribute additional unnecessary truck trips and associated air quality, noise, and traffic impacts to the neighborhood.

2. The granting of the application is necessary for the preservation and enjoyment of substantial property rights of the subject property's owner;

Property Right Impacts due to Protected Use Requirements - The Zoning Ordinance creates an impossible threshold for replacement of protected uses by requiring replacement at time of demolition as opposed to at the time of occupancy. In order to develop this site, the entire property must be cleared at one time – temporarily eliminating the protected use floor areas. The project and associated public improvements to Fourth Street and Bancroft Way are otherwise infeasible.

The required protected use area will be replaced in the new project for the life of the project and subject to the MU-LI protected use requirements. Specifically, the project permanently retains at least 29,347 square feet of protected manufacturing use space as well as protected warehouse space (converted to Research and Development use as allowed by the Zoning Ordinance) in the new building during operation of the project.

Retention of Protected Area During Construction Not Feasible - One option analyzed by the project team is the retention of the protected use space during construction by phasing the project. This option would add significant project costs due to construction inefficiencies, anticipated cost escalation, and lost revenue due to timing of marketing and generating leasing revenues. Phased grading and utility connections is inefficient and infeasible. The team explored locating a temporary building on the site during construction, but this would entail hooking up utilities, working around the building for site work, at substantial cost.

The requirement to find replacement space prior to demolition of the projected use will render the project infeasible, thereby, negating the owners' preservation and enjoyment of substantial property rights.

3. The establishment, maintenance or operation of the use or the construction of a building, structure or addition thereof, to be approved will not, under the circumstances of the particular case, materially affect adversely the health or safety of persons residing or working in the neighborhood of the property of the applicant and will not, under the circumstances of the particular case, be materially detrimental to the public welfare or injurious to property or improvements in said neighborhood; and that the granting of the Variance will promote the

municipal health, welfare and safety and benefit the City as a whole;

Achieving Intent of Protected Use Replacement - The variance request results in a temporary gap from the protected use replacement requirements. Once the building is occupied, the protected use area will be reestablished. The new space will be modern and will last long into the future. The same is not true of the existing manufacturing and warehouse space on the site. As a result, the replacement of protected space within the project in the long-term meets the intent of the West Berkeley Plan, MU-LI District purposes, and the requirements of this finding, while allowing the upgrades associated with redevelopment. The protected use space will be replaced in the new project, specifically as a condition of the new building's occupancy permit.

Neighborhood Benefit - The project would complement and be compatible with surrounding uses, which include warehousing, manufacturing, office, research and development, and laboratories. The project would further contribute to the economic and land use diversity described in the MU-LI district purposes. It would strengthen the Fourth Street retail district and provide a thriving work environment.

In these ways, the variance can help support the following purposes of the MU-LI district:

- B. Encourage development of a mixed use-light industrial area for a range of compatible uses;*
- E. Provide for the continued availability of manufacturing and industrial buildings for manufacturing uses, especially of larger spaces needed by medium sized and larger light manufacturers;*
- G. Provide the opportunity for laboratory development in appropriate locations;*

The project will provide a number of community benefits that contribute to the finding of non-detriment described above. Coordinated streetscape improvements along both sides of Fourth Street, and on Bancroft Way improve pedestrian connections. Pedestrian improvements extend to the UPRR tracks and will connect with new pedestrian sidewalk access being built by the Berkeley Commons/600 Addison Street project along Bolivar Drive. This will enable pedestrians to walk on the north side of Bancroft Way all the way to the Aquatic Park, which is not currently possible. Along the project frontage, a mid-block crosswalk improves walkability close to the Fourth Street retail corridor. A new public parklet and street trees provide opportunities for shade and public open space along Fourth Street. Expanded stormwater retention improves on-site management of run-off and improves water quality in the Aquatic Park lagoon and San Francisco Bay.

- 4. Any other variance findings required by the Section of the Ordinance applicable to that Variance.*

No other findings are required for this request.

ENVIRONMENTAL CONDITIONS

Key environmental topics that may be considered in the CEQA analysis include:

Historical & Cultural Resources:

As part of the project's historic resource evaluation, the building located at 703 Bancroft Way was found to be potentially eligible of the California Register. The significance was structural rather than architectural and the primary character-defining feature was the interior steel framework rather than the exterior of the building. As discussed in detail above, after initially planning to relocate the building on site, it became infeasible to do so after receiving significant fire, life, safety comments from Building and Safety Division and Berkeley Fire Department staff.

No known archeological sites extend into the project site. However, known sites, such as the West Berkeley Shellmound and intact midden deposits, are located in close proximity. Therefore, there may be sensitive cultural materials subsurface within the boundaries of the project site. The southwest corner of the parcel once housed a 19th Century rail depot and there were several residential dwellings on the property during the early 20th Century, which could contain cultural deposits pertinent to the history of early Euro-American settlement. Because of this cultural sensitivity, the project team's consulting archeologists recommend preconstruction testing or monitoring during ground-disturbing construction, especially at the southwest corner of the parcel.

Hazardous Materials: Previous on-site and off-site uses have contributed to the presence of hazardous materials in the soil and groundwater at the site. These are summarized below and documented in the Phase I report attached to this statement. The project will follow City and CEQA required mitigations to manage hazardous materials during construction and operation of the project.

There are elevated concentrations of various petroleum hydrocarbon (TPH) compounds, semi-volatile organic compounds (SVOCs), and volatile organic compounds (VOCs) and pesticides in the groundwater below the site from a previous pest control business. The project team's environmental consultants believe that that existing site and subsurface conditions (e.g., concrete floor slab and foundation system) are effectively limiting the mobility of the groundwater plume and find that the plume is stable. Therefore, in their present state, residual impacts present below the site do not appear to be posing a significant risk to human health and the environment.

The former Veriflo Facility (2246 Fifth St., off-site) is located approximately 200 feet east-southeast of the subject site and operated as a metals-finishing plant. Historical metals finishing operations at this facility resulted in the release of VOCs, primarily trichloroethene (TCE), tetrachloroethene (PCE), and vinyl chloride (VC). Investigations indicate groundwater impacts have migrated downgradient and offsite towards the northwest, which is in the direction of the subject site.

Several of the subject site's facilities currently and/or historically used chlorinated solvents, hazardous materials, and petroleum products. No spills or evidence of any releases were reported for current or historical facilities. However, the current and historical uses of these chemical products at the subject site may have contributed to impacts identified in the Phase I report.

Soils/Geotechnical Findings: The site is blanketed by medium stiff to very stiff clay fill with variable sand and gravel content. The fill is underlain by native alluvium consisting of interbedded layers of stiff to hard clay with varying sand content and medium dense to very dense clayey sand with varying gravel content. Near surface soil may be highly to very highly expansive. The project team's geotechnical analysis indicates the underlying soils below the groundwater are not susceptible to liquefaction because of their cohesion. The analysis indicates clay layers at deeper depths may experience cyclic softening during a major earthquake event but concludes the risk of lateral spreading to be nil. Following site demolition, the existing fill below the proposed building would be over-excavated and recompacted. Foundations and slabs would be designed and constructed to resist the effects of the expansive clay.

Traffic: A Traffic Impact Analysis has been completed and is part of the Use Permit application. As outlined above, the project proposes a robust TDM program and improvements that aim to further reduce vehicle trips and promote the use of alternative modes of transportation.

787 Bancroft Way: Design Narrative

theLAB Berkeley, Phase III

1st Design Review Committee Meeting | February 4, 2022



Our goal is to introduce a new building which transforms and enriches the existing urban fabric through careful spatial interventions and a straightforward, humble materiality.



Enrich the Urban Fabric
787 Bancroft



Connect Office to Open Space
North Plaza



Reflects the Neighborhood Character
theLAB Phase I & II

Project Vision

The Lab is a multi-phase combination of adaptive reuse, increased open space, new construction, and enhancements to the public realm will improve the environment and support the development of businesses in the surrounding neighborhood.

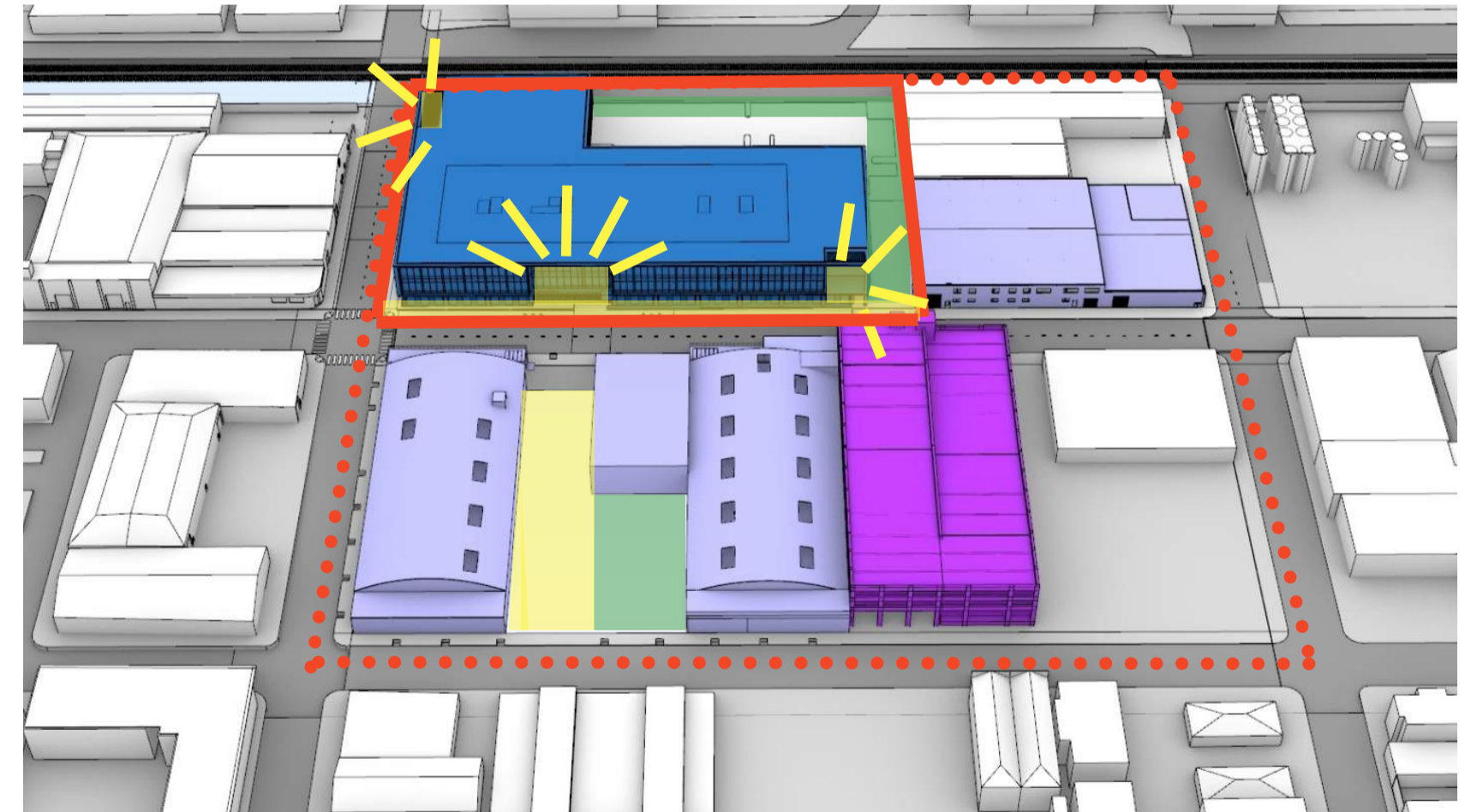
The building's L-shaped configuration and long horizontal proportions maintain the form and character of the surrounding blocks. With two major frontages, the building retains the street walls on both Fourth Street and Bancroft Way while allowing for a generous public open space to the north of the building and public improvements along Fourth Street.

The form and materiality of the building recall the simplicity and honesty of the industrial buildings in the neighborhood while making the block more welcoming and the pedestrian experience more enjoyable. A combination of exposed concrete structure, glass, metal trim, and integrated planters complement the industrial character of the district, while providing new open spaces to the evolving neighborhood and its more pedestrian and bicycle friendly orientation.

The ground floor is slightly recessed to increase the sidewalk width for pedestrian movement and additional planting and seating. The recessed ground level gives the building the same strong horizontal proportions as the previous building, despite being one story taller. The entry and address sign are near the midpoint of the Fourth Street façade, breaking up the long mass on the ground floor.

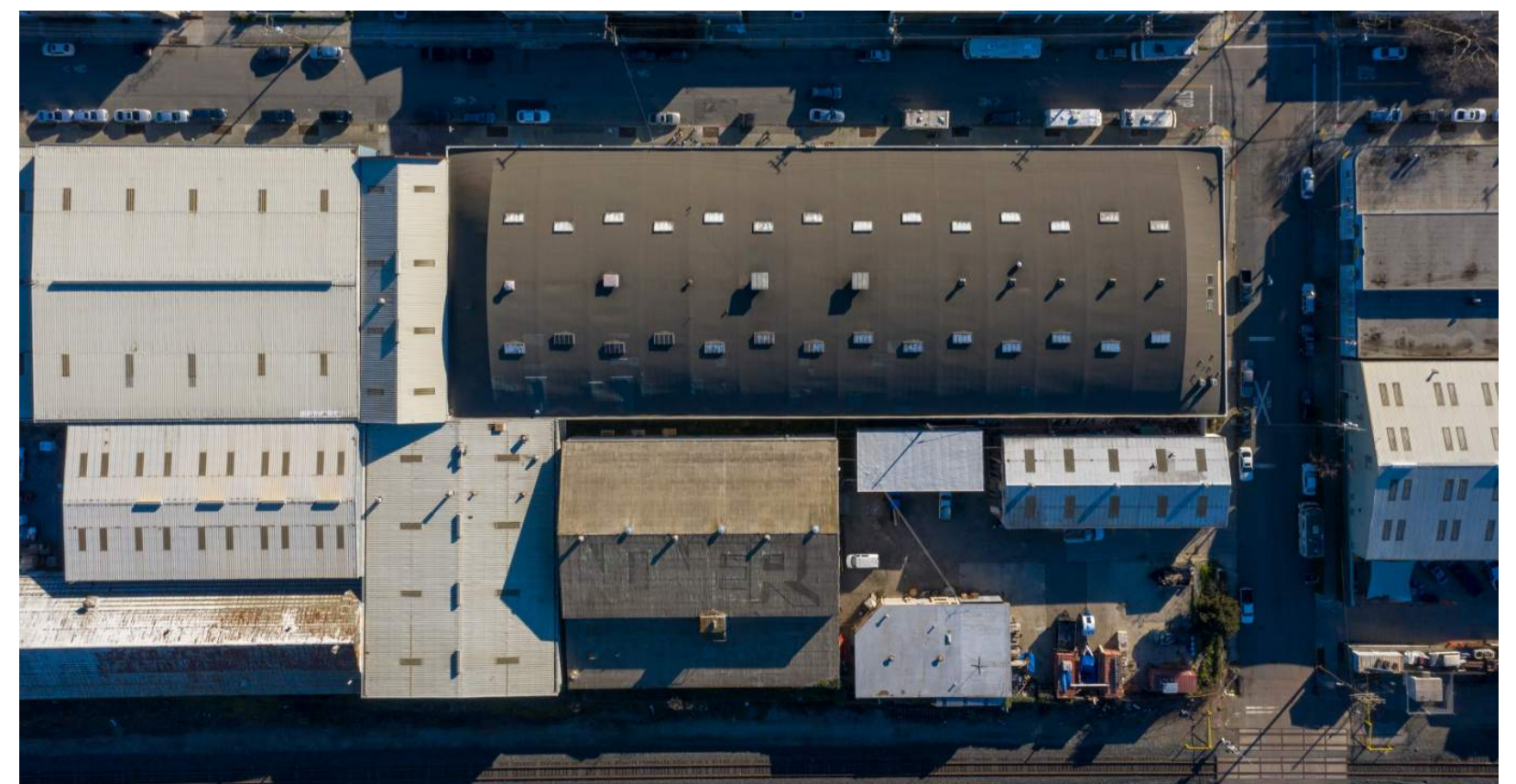
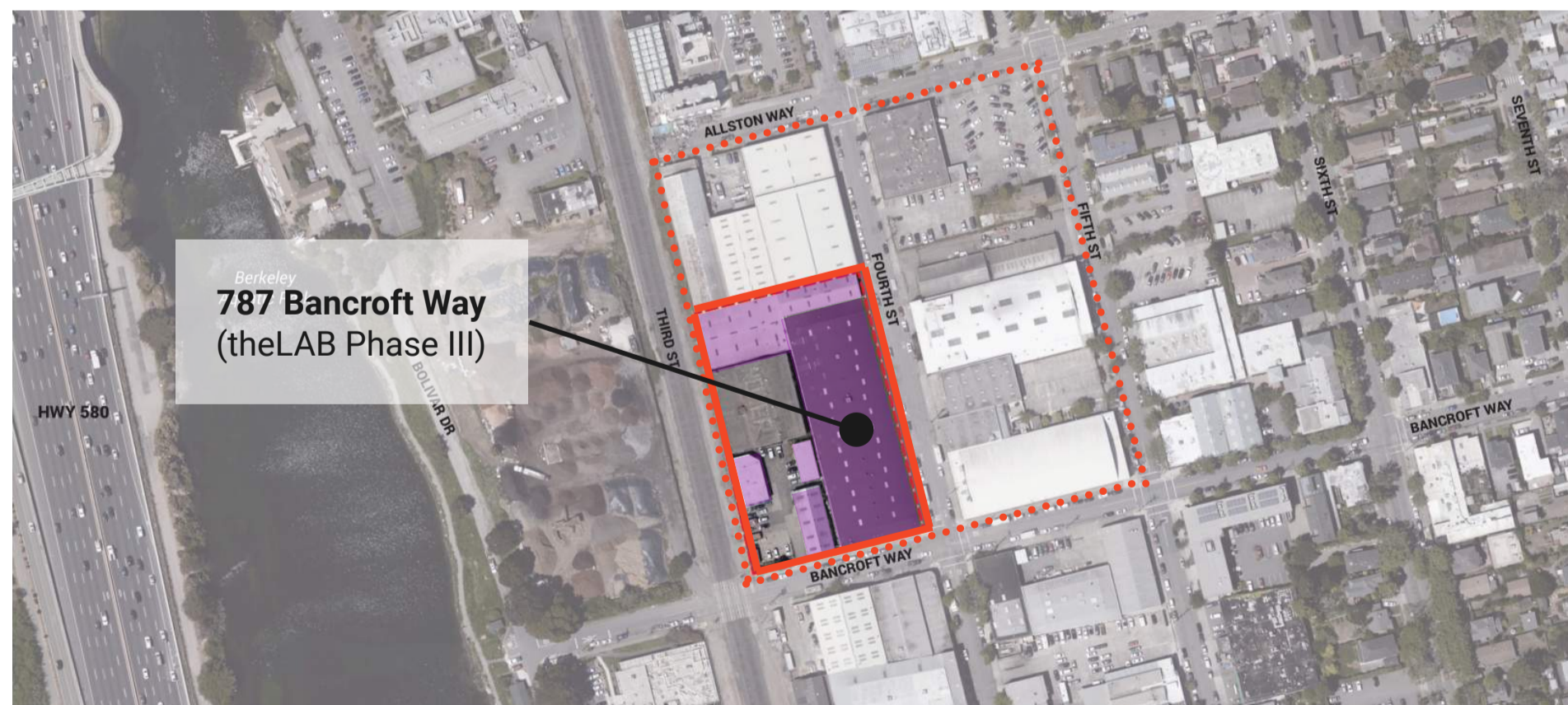
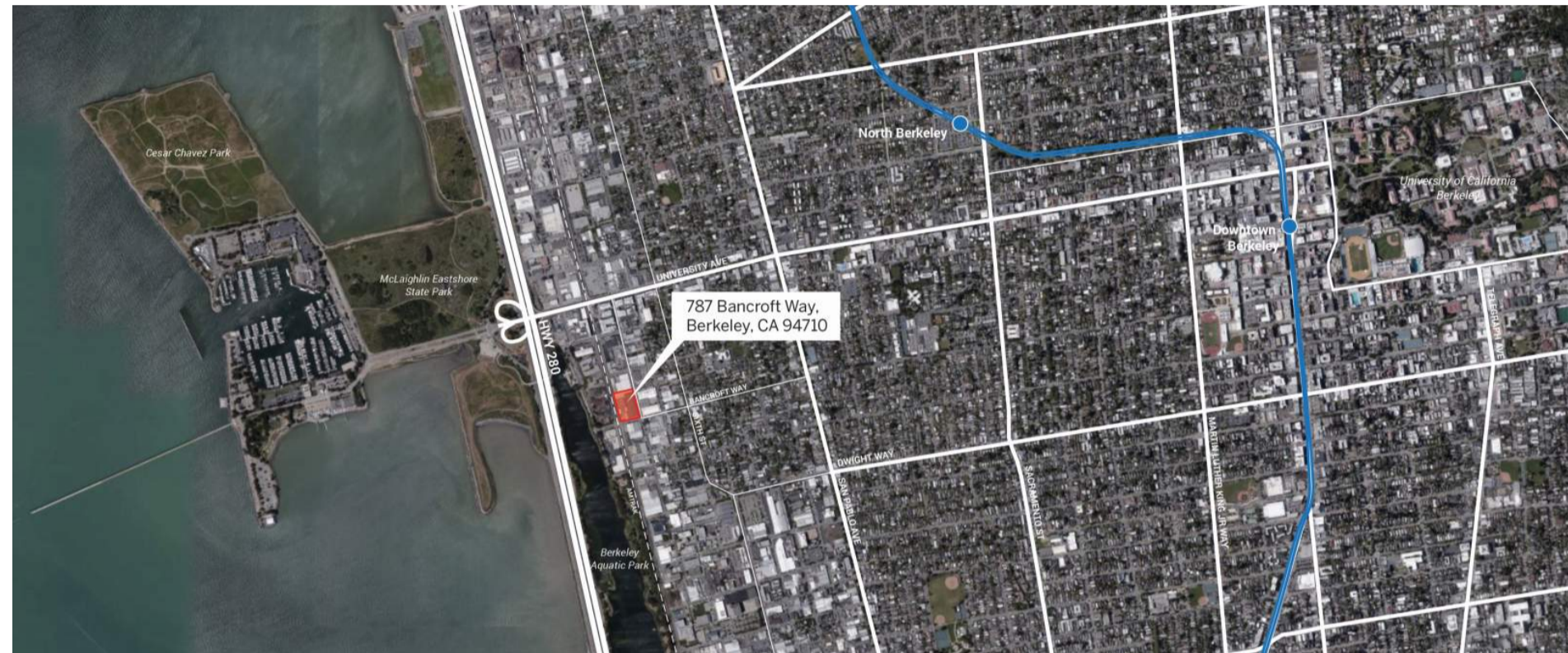
The entry is aligned with a new open space across the street that ties all of the buildings of TheLab into a singular campus-like work environment. Terraces at the Southwest and Northwest corners of the building break down the mass at the corners, provide tenants and outdoor amenities with views, and emphasize the connection between the workplace and neighborhood. The external stair on the Northeast corner provides vertical circulation and further connectivity between the workplace and Northern open space.

The pedestrian entrances to the site are located off Fourth Street. Vehicular access to the site is provided on Bancroft Way. This entry location reduces the curb cuts along Fourth Street to enhance the pedestrian experience and connectivity between buildings on either side of the street.



WEST BERKELEY

The project is located in West Berkeley - a neighborhood with a diverse range of uses, building types, scales, and a rich industrial past. 787 Bancroft and the Lab will help the neighborhood evolve into the next chapter in the area's economic growth through a combination of new construction and adaptive reuse.

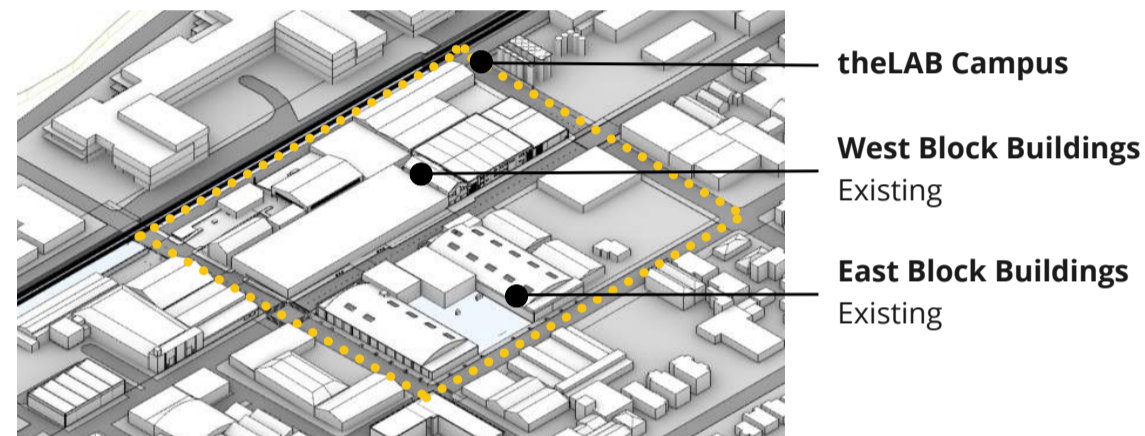


theLAB Phases

787 Bancroft is the last phase of a multi-phase development that creates a light manufacturing and R&D campus in West Berkeley. The replacement of underutilized structures with a new building dedicated to R&D and light manufacturing uses serves the purpose of fostering growth of advanced technology services, while encouraging the availability of buildings for manufacturing uses and jobs. A series of public realm improvements and small scale parks stitch the project into the existing fabric of the neighborhood.

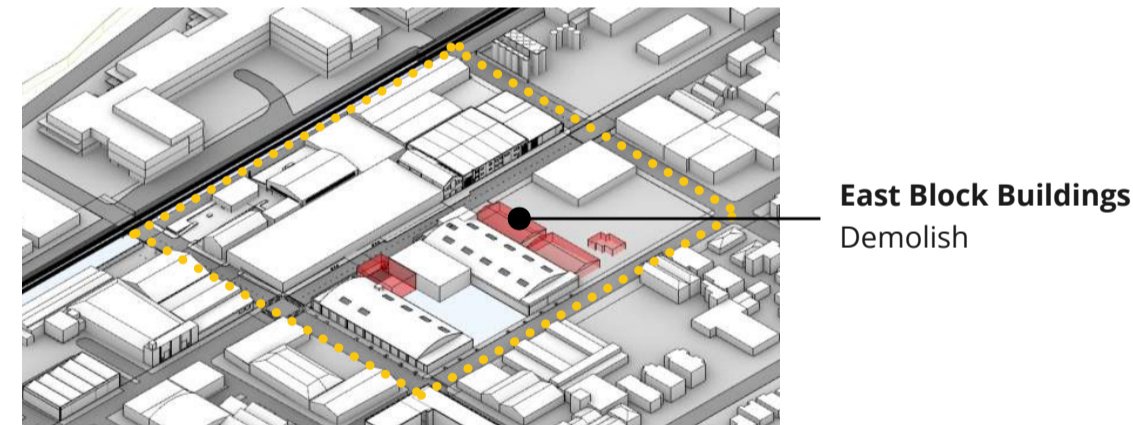
PREVIOUS PHASES

PHASE 0 - EXISTING CONDITION



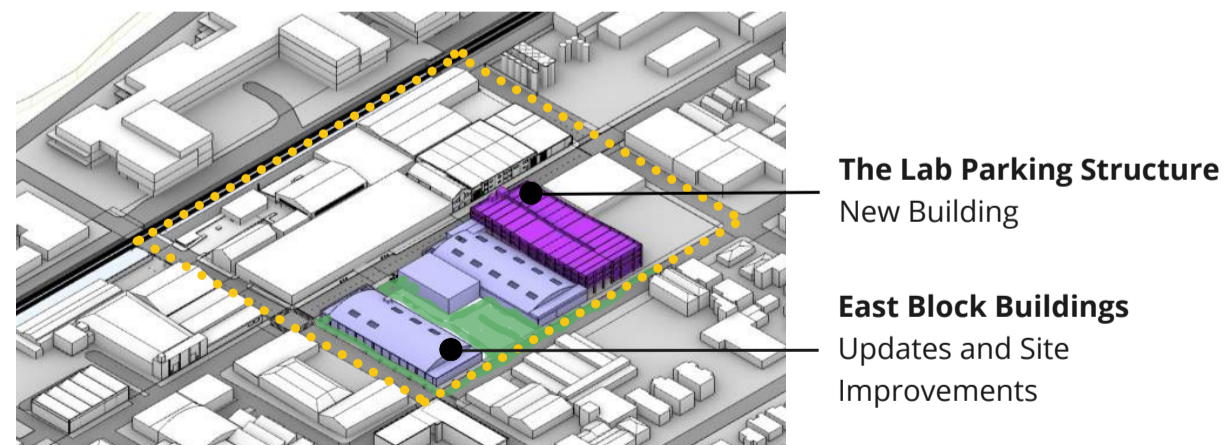
PHASE I

- Demolish 4 Structures on East Block



PHASE II

- The Lab Parking Structure - Build New
- East Block Buildings - Updates

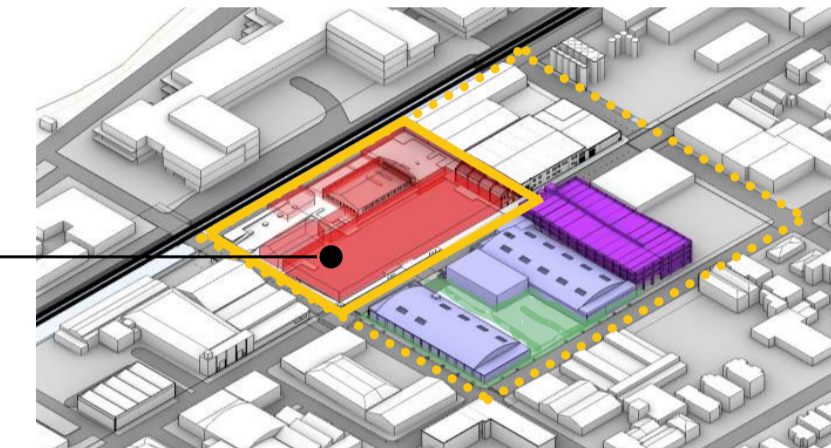


CURRENT SUBMITTAL PHASE

PHASE III - A

- Demolish Structures on West Block

West Block Buildings Demolish

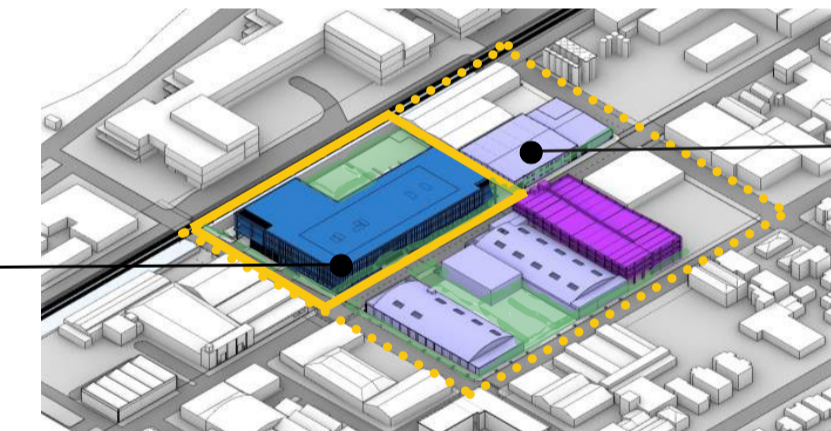


PHASE III - B

- 787 Bancroft - Build New
- 716 Allston - Update

787 Bancroft New Building and Site improvements

716 Allston Update



BUILDING PLACEMENT

The L-shape configuration of 787 Bancroft maximizes the building frontage along Bancroft and 4th street, creates open space adjacent the public right of way, and discreetly places parking on the interior of the site. Our team explored a variety of building configurations and placement. The L-shape configuration created the best pedestrian experience along 4th street.



the square

compact plan

parking & additional curb cut along 4th



the rectangle

strong connection to 4th street & East development

no open space on the site

east west solar orientation

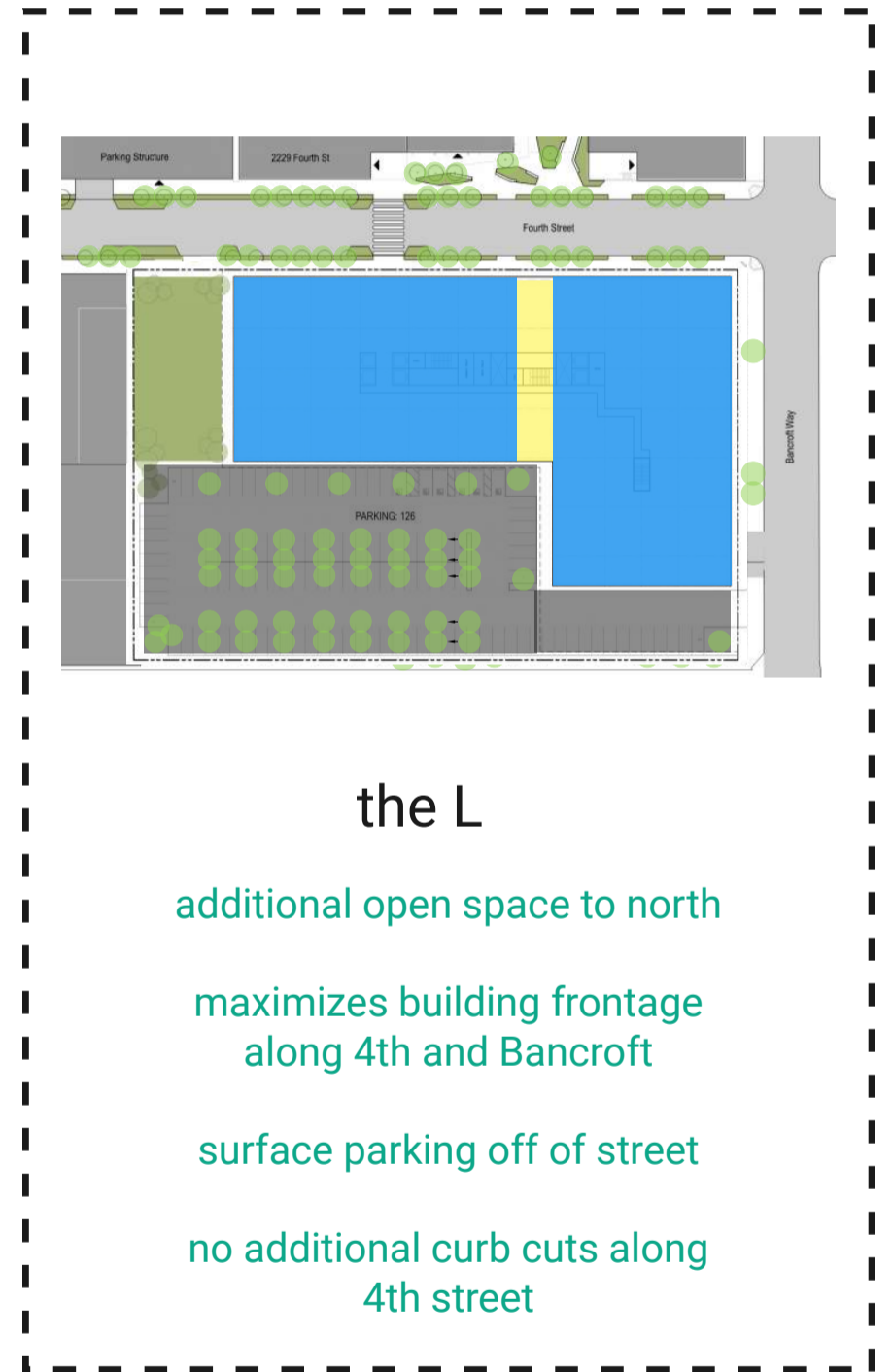


the U

good views

open space isolated

parking & additional curb cut along 4th



the L

additional open space to north

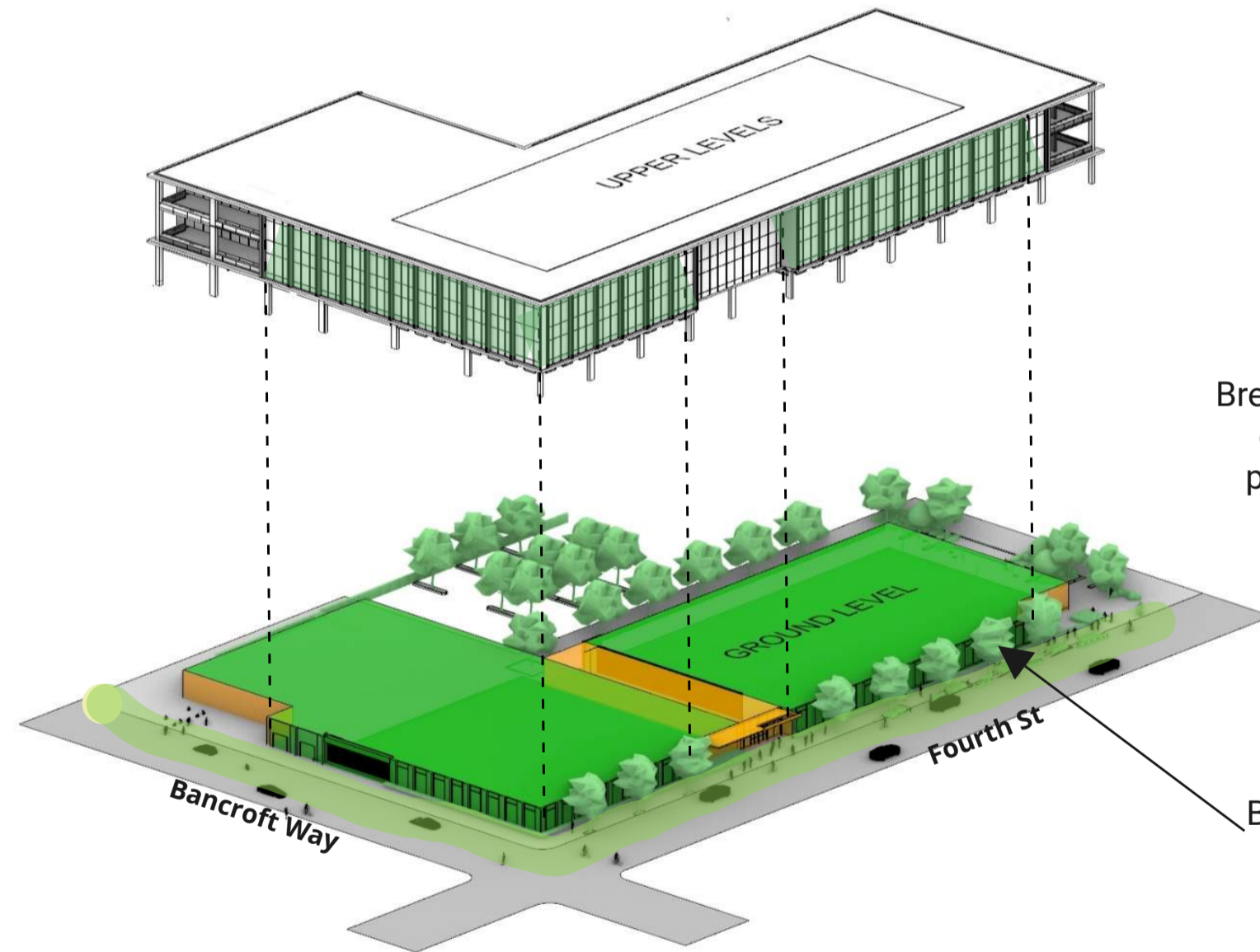
maximizes building frontage along 4th and Bancroft

surface parking off of street

no additional curb cuts along 4th street

BUILDING MASSING AND ARTICULATION

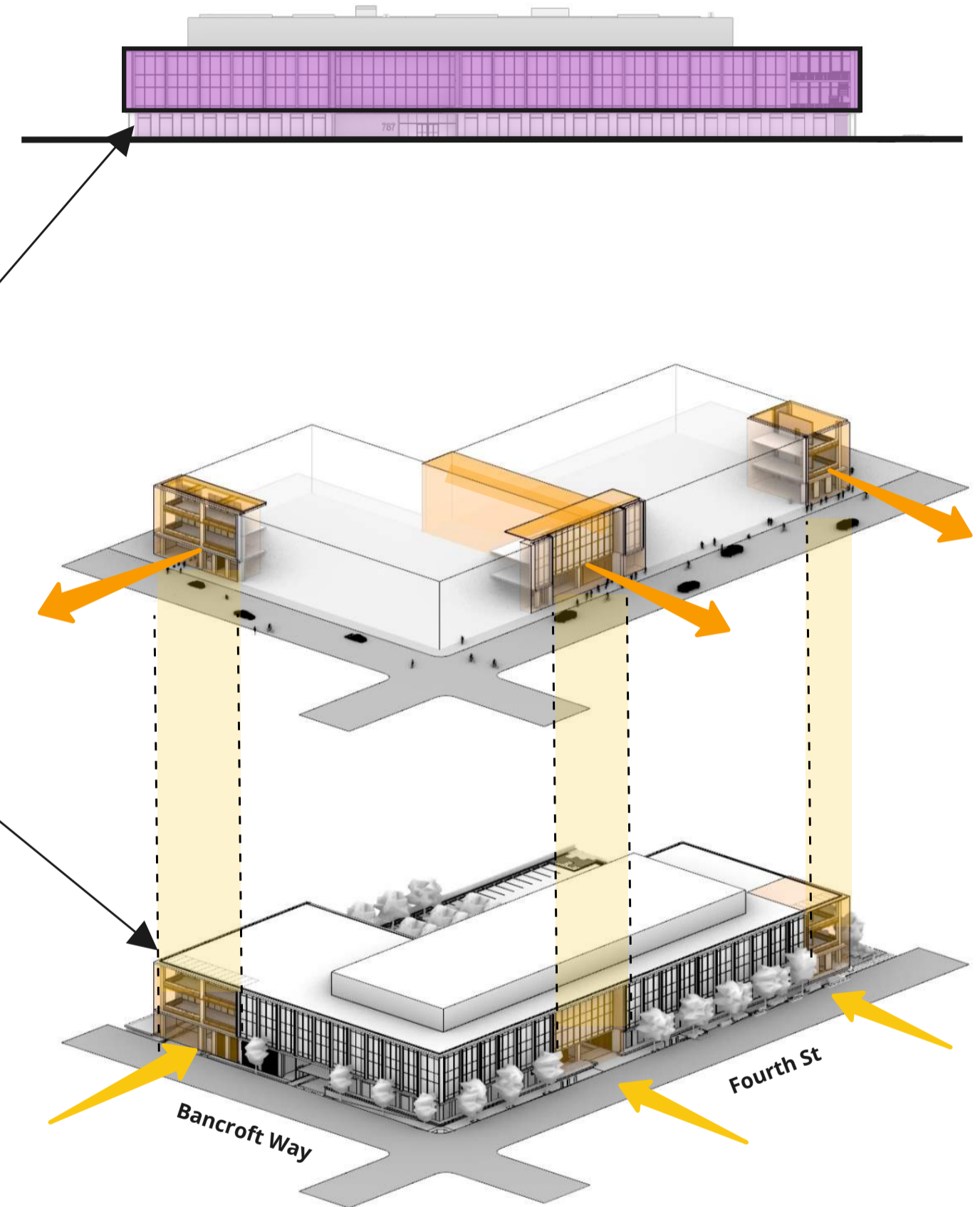
The form of 787 shares the long and low proportions of the other revitalized warehouses with the Lab to create a cohesive building language within the masterplan. The ground floor is recessed to allow for increased landscaping along the sidewalk and for the scale of the building to be broken down. A larger recess at the building entrance is near the center of the building to break down the long mass and align the building to a new open space across 4th street. Large outdoor terraces on both ends of the building break down the scale of the building next to its neighbors. An exterior stair on the Northeast corner creates an active circulation area adjacent to the public way and new Northern park.



OVERALL MASSING
Long and low profile with open
ground plane

ACTIVATED TERRACES
AND ENTRY
Breaks down elevation length and
engages workplace with the
public realm at parcel's edges

RECESSED GROUND LEVEL
Broken into two-building masses
to activate the sidewalk and
emphasize human scale

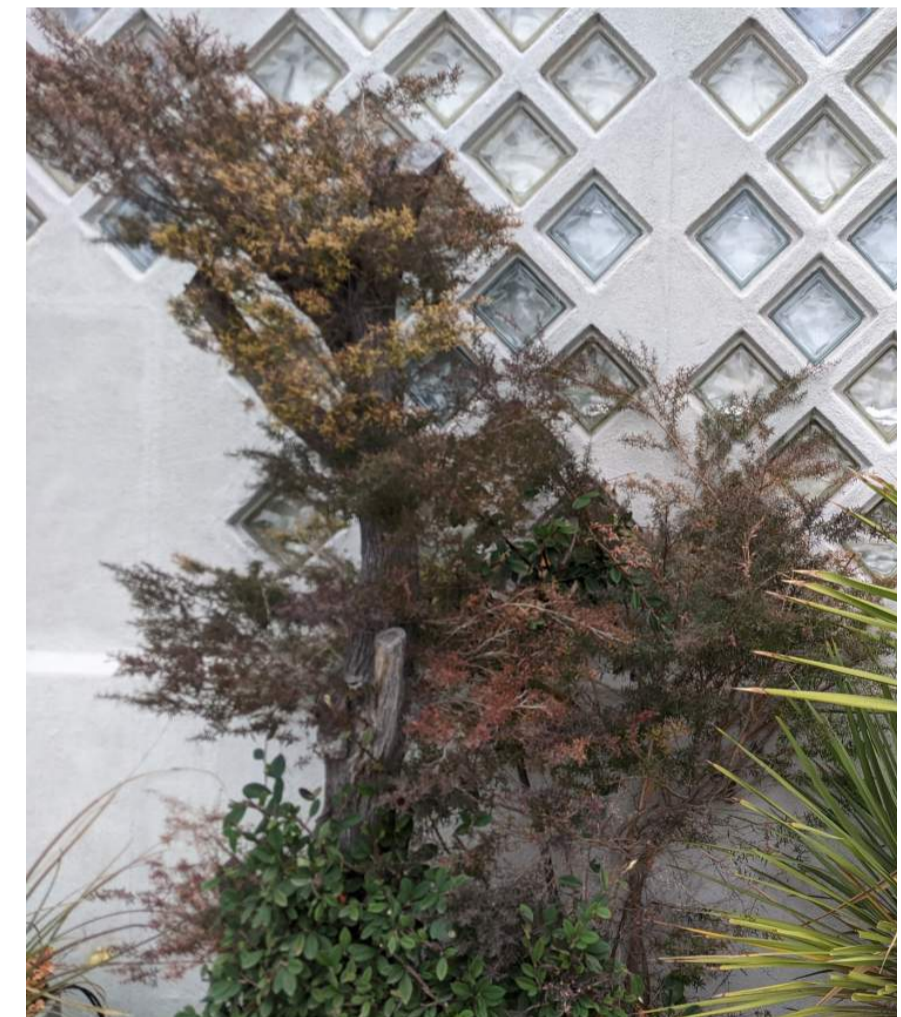


4TH STREET FRONTAGE



MATERIALITY AND CHARACTER

The construction and materiality building is inspired by the functionality and playfulness of the industrial buildings in the neighborhood. There is a clear expression of the structural frame and a play in rhythm of the cast in place concrete that is also the primary building facade.



MATERIALITY AND CHARACTER

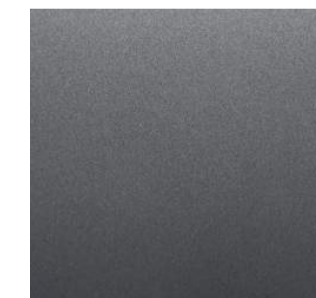
The construction and materiality building is inspired by the functionality and playfulness of the industrial buildings in the neighborhood. There is a clear expression of the structural frame and a play in rhythm of the cast in place concrete that is also the primary building facade.



AC-1
Smooth Structural
Concrete



GL-1
Low Iron Glass



FIN-1
Gray Metal Panel



MTL-4
Corrugated Metal
Panel

4TH STREET



4th Street Entrance



4th Street Crosswalk

BANCROFT WAY



Southwest Corner on Bancroft



Southeast Corner at 4th and Bancroft

OPEN SPACE AND LANDSCAPE

The landscape design strategy for 787 Bancroft and the Lab Masterplan is to transform the two blocks East and West of 4th Street into a singular campus consisting of a variety of open spaces and outdoor experiences. This is done through a series of interventions: Widening the sidewalk for better pedestrian experiences, creating pocket parklets along the street for light activities, adding a mid-block crossing to enhance the Lab campus connectivity, and finally, providing a shaded mixed-use plaza for the workers to extend their daily activities from indoor to outdoor.

The North Plaza acts as a crossroads for pedestrian circulation between 787 Bancroft, 716 Allston, and 4th street. It is also a key outdoor area of the site. The park has a variety of uses for both the occupants of the Lab and the public. There is seating under the shaded London Plane trees and the Japanese Maples add a layer of delicacy to the gardens. The open lawn provides a gathering space that enriches the work environment. Both the design of North Plaza and the improvements of 4th street are to establish a sense of place in this new vibrant hub for the district.



1. 4th Street Sidewalk



2. North Park



OPEN SPACE AND LANDSCAPE

The planting design of 787 Bancroft takes the neighborhood context, the relationship with the main building, and the seasonal plant characters into consideration.

Red Maple is selected as the street tree along 4th street to create a unified scene across the Lab campus. Its brilliant fall color and clean oval tree shape will give a distinct identity to the site within the neighborhood and marks the front entrances for the buildings along 4th Street.

In the North Plaza three London Plane trees are placed to provide great shade in summer and visual interests in winter. Japanese Maples are also designed into the plaza gardens. With its delicate leaves and outward spreading branches, these garden trees easily become the visual attraction on site.

In the surface parking area behind the building, medium sized Frontier Elms are used to provide shade for the cars while not blocking too much of the view towards the ocean.

All groundcovers and low plantings will be native or drought-tolerant plants that require low water and less maintenance once established. Plants vary from colors, heights, forms, and textures in different seasons and add year-round visual interest.



Red Maple
Acer rubrum "October Glory"



Japanese Maple
Acer Japonica



London Plane Tree
Platanus hybrida



Frontier Elm
Ulmus Hybrid "Frontier"

SITE FEATURES

SIDEWALK EXPANSION

The design of the 4th Street streetscape activates the public realm of the block with strategic planting and landscape features that are coordinated with major building elements. Red Maple is selected as the street tree marching along both sides of the street, unifying the campus. With their orange to brilliant scarlet fall color, the red maples contribute to the planting diversity of the neighborhood and create identity for the Lab campus. The Sidewalk is widened to incorporate three sets of pocket parklets with gardens and seating benches. Hedges are designed to provide protection from the traffic and comfort to the parklets. Both the layouts of the street trees and pocket parklets are responding to the rhythm of the building façade. Along the periphery of the main building, a continuous flow-through planter has been established to help with storm water management as well as create a green buffer between the office environment and public sidewalk.



SECURITY GATE

The design of the West Block includes security gates on the NE plaza and the SW vehicular entry to the site. The gates will remain open during operating hours of the day in order to keep the Northeast plaza connected to the public realm. Signage will be integrated into the gates in harmony with the project landscape and architecture.



OPEN SPACE PLANTING AND MATERIAL

All groundcovers and low plantings will be native or drought-tolerant plants that require low water and less maintenance once established. Plants vary from colors, heights, forms, and textures in different seasons and add year-round visual interest.



California Privet
Ligustrum
Ovalifolium



Carpet Manzanita
Arctostaphylos
'Emerald carpet'



Fountain Grass
Pennisetum
alopecuroides



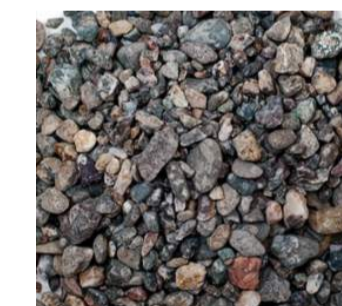
Berkeley Sedge
Carex tumulicola



P2A
Concrete Paver
Natural



P2B
Concrete Paver
Limestone Gray



P6
Crushed Granite



W2
Bench cap stone
Sierra white, thermal
top spilt edge

TREE COVERAGE



BIRD MITIGATION STRATEGIES

The new project is 350 feet from the Aquatic Park and 1,000 ft from the San Francisco Bay. Both are major open spaces along the Pacific Flyway: a major migratory route for birds. Between our project and these open spaces will be Berkeley Commons another project of similar height.

While we are not directly adjacent to these open spaces we are taking the following measures to reduce the likelihood bird collisions:

Bird Safety Measures (per BMC amendment):

1. Recessed Glazed Surfaces
2. Glazing that minimizes reflectivity through metal panels and decorative fins
3. Parking Lot Tree Canopy between open space and building
4. Light-colored blinds or curtains



Project site distance from Aquatic Park



1 and 4: Recessed windows at West facade with light colored curtains



3: Parking lot tree canopy in front of inner west facade



2: Inner West facade solid metal panels and decorative fins



VIEW FROM TRAIN TRACKS



NORTHEAST CORNER

